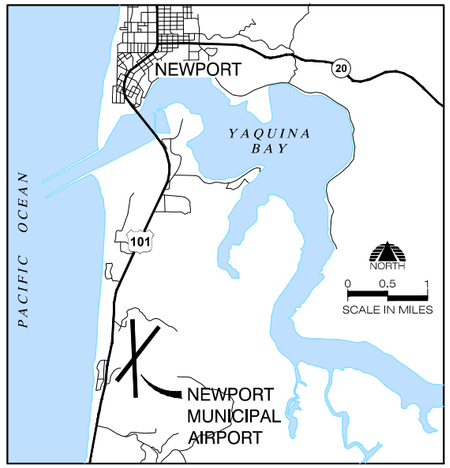
NEWPORT MUNICIPAL AIRPORT

RUNWAY 16-34 REHABILITATION
AIP NO. 3-41-0040-021

VICINITY MAP



INDEX TO DRAWINGS

PRECISION APPROACH ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
541•754•0043

REGISTERED PROFESSIONAL ENGINEER
72693PE
OREGON
JUNE 14, 2007
GREOFF W. VAUGHN
EXPIRES 6/30/2014

REVISIONS: DATE APPD.

DATE: JUNE 2013
DESIGN: TLK
DRAWN: JAW
CHECKED: GWV
REVISION NUMBER: 0

SCALE: AS SHOWN

NEWPORT MUNICIPAL AIRPORT
RUNWAY 16-34 REHABILITATION

COVER SHEET

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: **NPT008**

SHEET NO. **1** OF **108**

BASE BID SCHEDULE ITEMS
CONSTRUCTION YEAR 2013

BID ITEM NUMBER	DESCRIPTION	UNIT	ESTIMATED QUANTITY	CONSTRUCTION QUANTITY
1	MOBILIZATION/DEMOBILIZATION	LS	1	
2	CONSTRUCTION SURVEY	LS	1	
3	DEMOLITION	LS	1	
4	PAVEMENT CRACK SEALING	LF	5,500	
5	PAVEMENT MARKING REMOVAL	SF	6,200	
6	ASPHALT PAVEMENT REMOVAL	SY	93,840	
7	ASPHALT GRINDING PLACEMENT	SY	51,600	
8	ASPHALT GRINDING DISPOSAL	TON	15,420	
9	GEOTEXTILE FABRIC	SY	31,610	
10	UNCLASSIFIED EXCAVATION	CY	79,350	
11	UNSUITABLE EXCAVATION	CY	11,910	
12	SUBBASE COURSE	CY	12,890	
13	TEMPORARY EROSION CONTROL	LS	1	
14	SILT FENCE	LF	2,759	
15	CRUSHED AGGREGATE BASE COURSE	CY	13,080	
16	BITUMINOUS SURFACE COURSE	TON	19,330	
17	BITUMINOUS PRIME COAT	TON	70	
18	BITUMINOUS TACK COAT	TON	25	
19	PAVEMENT MARKING, WHITE/YELLOW	SF	91,220	
20	PAVEMENT MARKING, BLACK	SF	2,520	
21	PAVEMENT MARKING, THERMOPLASTIC	SF	2,470	
22	6-INCH SOLID HDPE PIPE	LF	2,780	
23	12-INCH REINFORCED CONCRETE PIPE	LF	4,620	
24	18-INCH REINFORCED CONCRETE PIPE	LF	2,900	
25	24-INCH REINFORCED CONCRETE PIPE	LF	1,880	
26	30-INCH REINFORCED CONCRETE PIPE	LF	620	
27	PIPE OUTFALL "F" RECONSTRUCTION	LS	1	
28	27-INCH CURED-IN-PLACE PIPE (CIPP)	LF	263	
29	30-INCH CURED-IN-PLACE PIPE (CIPP)	LF	123	
30	6-INCH PERFORATED HDPE PIPE	LF	12,893	
31	STORM DRAIN CATCH BASIN	EA	27	
32	STORM DRAIN MANHOLE, 48-INCH	EA	30	
33	STORM DRAIN MANHOLE, 60-INCH	EA	4	
34	STORM DRAIN OUTFALL STRUCTURE	EA	3	
35	CLEANOUT	EA	50	
36	UNDERDRAIN ACCESS STRUCTURE	EA	9	
37	1/C NO. 8 CU (5KV) CABLE	LF	55,800	
38	L-828, 7.5KW MIRL REGULATOR	LS	1	
39	L-828, 20KW HIRL REGULATOR	LS	1	
40	ELECTRICAL SERVICE UPGRADE	LS	1	
41	MISCELLANEOUS ELECTRICAL BUILDING IMPROVEMENTS	LS	1	
42	ELECTRIC SERVICE FOR AWOS AND WIND CONE	LS	1	
43	2-INCH PVC CONDUIT	LF	38,500	
44	CONCRETE ENCASEMENT OF DUCT	LF	900	
45	TRENCH, NON-PAVED AREA	LF	30,500	
46	TRENCH, STRUCTURAL PAVED AREA	LF	300	
47	TRENCH, NON-STRUCTURAL PAVED AREA	LF	500	
48	HANDHOLE	EA	26	
49	JUNCTION CAN	EA	34	
50	RUNWAY EDGE/THRESHOLD LIGHT, ELEVATED	EA	74	
51	RUNWAY EDGE LIGHT, IN-PAVEMENT	EA	2	
52	TAXIWAY EXIT LIGHT, ELEVATED	EA	12	
53	GUIDANCE SIGN, 1 MODULE	EA	6	
54	GUIDANCE SIGN, 2 MODULE	EA	14	
55	GUIDANCE SIGN, 3 MODULE	EA	13	
56	GUIDANCE SIGN, 4 MODULE	EA	1	
57	DISTANCE REMAINING SIGN	EA	4	
58	RETROREFLECTIVE MARKER	EA	50	
59	HYDROSEEDING	AC	36	
60	IMPORTED TOPSOIL	SY	28,112	
61	FAA EQUIPMENT DEMOLITION	LS	1	

BASE BID SCHEDULE ITEMS
CONSTRUCTION YEAR 2014

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57	DISTANCE REMAINING SIGN	EA	4	
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59	HYDROSEEDING	AC	36	
60	IMPORTED TOPSOIL	SY	28,112	

ADDITIVE BID SCHEDULE ITEMS
CONSTRUCTION YEAR 2013

BID ITEM NUMBER	DESCRIPTION	UNIT	ESTIMATED QUANTITY	CONSTRUCTION QUANTITY
A1	RELOCATE AND ENCLOSE EXISTING 35KW GENERATOR	LS	1	
A2	NON-AIP ELIGIBLE TERMINAL BUILDING ELECTRICAL IMPROVEMENTS	LS	1	
A3	NEW 100KW GENERATOR	LS	1	

ADDITIVE BID SCHEDULE ITEMS
CONSTRUCTION YEAR 2014

BID ITEM NUMBER	DESCRIPTION	UNIT	ESTIMATED QUANTITY	CONSTRUCTION QUANTITY
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RUNWAY 16-34 REHABILITATION

BID SCHEDULE

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: **NPT008**

SHEET NO. **2** OF **108**

LEGEND

CIVIL SYMBOLS LEGEND

EXISTING	NEW	
		EDGE OF PAVEMENT
		EDGE OF GRAVEL
101.8	101	CONTOUR
		DITCH FLOW LINE
X X	X X	FENCE
SD SD	SD SD	STORM DRAIN
UD UD	UD UD	UNDER DRAIN
		CULVERT
SS SS	SS SS	SANITARY SEWER
W W	W W	WATER
G G	G G	GAS
T T	T T	TELEPHONE
IRR IRR	IRR IRR	IRRIGATION
		SLOPE SYMBOL
		CATCH BASIN
SD	SD	STORM DRAIN MANHOLE
		CLEANOUT
		CULVERT END STRUCTURE
		SURFACE FLOW DIRECTION
SS	SS	SANITARY MANHOLE
		FIRE HYDRANT
W	W	WATER METER
W	W	WATER VAULT
		WATER VALVE
		IRRIGATION VALVE
		TIE DOWN
		SIGN
		SURVEY CONTROL POINT
		SURVEY FOUND MONUMENT
		BRASS CAP
		BENCHMARK
		AIRPORT BEACON
		SHRUBS
		TREES
		ABANDONED FEATURE LINE
		DEMO FEATURE LINES

ELECTRICAL LEGEND

EXISTING	NEW	
H	H	HANDHOLE
P	P	POWER VAULT
DM	DM	CABLE/DUCT MARKER
J	J	JUNCTION CAN
E	E	ELECTRICAL RISER
		RUNWAY EDGE LIGHT AND THRESHOLD LIGHT C = CLEAR R = RED G = GREEN Y = YELLOW O = OPAQUE
		RUNWAY LIGHT IN PAVEMENT (FLUSH MOUNT)
		REIL (RUNWAY END IDENTIFIER LIGHT)
		PAPI (PRECISION APPROACH PATH INDICATOR)
		TAXIWAY LIGHT (BLUE LENS)
		TAXIWAY LIGHT IN PAVEMENT (FLUSH MOUNT)
		RETROREFLECTIVE MARKER
POH	POH	POWER CIRCUIT / OVERHEAD
PUG		POWER UNDERGROUND
		POWER POLE
	P	GUY WIRE
		STREET LIGHT
		GUIDANCE SIGN

GENERAL CONSTRUCTION NOTES:

1. THE AIRPORT SHALL REMAIN OPEN TO AIRCRAFT OPERATIONS AT ALL TIMES DURING CONSTRUCTION EXCEPT FOR ALLOWED CLOSURE PERIOD OF BOTH RUNWAYS AT THE SAME TIME.

2. WORK WITHIN THE RUNWAY OBJECT FREE AREA (ROFA) OR TAXIWAY OBJECT FREE AREA (TOFA) WILL REQUIRE CLOSURE OF THE RUNWAY OR THE TAXIWAY AS APPLICABLE UNLESS SPECIFIC CONDITIONS ARE MET. SEE NOTE 6. PLACEMENT OF CLOSURE CROSSES WILL BE REQUIRED ANYTIME THE RUNWAY IS CLOSED. THE OWNER SHALL BE NOTIFIED A MINIMUM OF 24 HOURS IN ADVANCE OF ANY REQUESTED CLOSURE. CLOSURE WILL REQUIRE APPROVAL BY THE OWNER AND AN APPROPRIATE NOTAM ISSUED BY THE OWNER.

3. ANY TIME THE CONTRACTOR OR HIS/HER PERSONNEL ARE INSIDE THE AIRPORT SECURITY FENCE THEY SHALL BE EQUIPPED WITH RADIOS CAPABLE OF RECEIVING AND BROADCASTING ON FREQUENCY 122.8. CONTRACTOR TO PROVIDE ADEQUATE NUMBER OF RADIOS APPROPRIATE FOR CONSTRUCTION ACTIVITY. THE CONTRACTOR SHALL OBTAIN TRAINING ON THE APPROPRIATE OPERATION AND PROTOCOL FOR BROADCASTING ON THE AIRPORT FREQUENCY.

4. RUNWAY CLOSURES WILL BE COORDINATED AS INDICATED IN CONSTRUCTION PHASE 1 AND 2 OF THE CONSTRUCTION SAFETY AND PHASING PLAN DRAWINGS. ACCESS TO THE TERMINAL BUILDING (FBO) AND GENERAL APRON AREA SHALL BE MAINTAINED TO THE MAXIMUM EXTENT POSSIBLE.

5. SEE DETAIL (2/9) AND SPECIFICATIONS, FOR RUNWAY CLOSURES.

6. IF THE OWNER AND FAA AGREE AND AN APPROPRIATE NOTAM IS ISSUED IT IS POSSIBLE THAT THE CONTRACTOR MAY WORK AS CLOSE AS 75 FEET FROM THE RUNWAY 16-34 CENTERLINE OR RUNWAY 2-20 CENTERLINE. SEE AC 150/5370-2F, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION AND THE SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS.

7. ALL VEHICLE DRIVERS NOT TRAINED ON AIRPORT OPERATIONS SHALL BE ESCORTED BY THE CONTRACTOR'S AUTHORIZED REPRESENTATIVE AT ALL TIMES WHEN IN AIRPORT OPERATIONS AREA. ALL PERSONAL VEHICLES SHALL REMAIN WITHIN THE CONTRACTORS STAGING AREA WHILE WITHIN THE AIRPORT SECURITY FENCE.

8. BARRICADES, LIGHTS, AND OTHER CONSTRUCTION CONTROL DEVICES FURNISHED, PLACED, AND MAINTAINED BY THE CONTRACTOR SHALL BE PROVIDED AT VARIOUS LOCATIONS, AS NECESSARY TO ADEQUATELY SEPARATE CONSTRUCTION ACTIVITIES FROM AIRCRAFT OPERATIONS AREA (AOA) OR OTHER ACTIVITIES IN THE AREA.

9. ALL ACCESS ROUTES SHALL BE MAINTAINED DURING CONSTRUCTION AND SHALL BE RESTORED TO ORIGINAL CONDITION.

10. ACCESS TO THE AIRPORT SHALL BE THROUGH DESIGNATED AUTOMATED VEHICLE GATES OR TEMPORARY GATE. ANY TIME AN AIRPORT SECURITY GATE REMAINS OPEN FOR CONSTRUCTION OPERATIONS, THE CONTRACTOR SHALL PROVIDE A GATE GUARD TO CONTROL ACCESS.

11. PRIOR TO REOPENING THE CLOSED RUNWAY, TAXIWAYS, OR GENERAL AIR OPERATIONS AREA (AOA) FOR OPERATIONS THE CONTRACTOR SHALL PROVIDE ADEQUATE TIME FOR THE ENGINEER OR OTHER AIRPORT REPRESENTATIVE TO INSPECT FOR CLEANLINESS AND CONFORMANCE TO REGULATIONS. THE CONTRACTOR SHALL ALSO VERIFY THAT THE RUNWAY LIGHTS AND OTHER NAVAID(S) ARE OPERATING PROPERLY. THE TIME NEEDED FOR INSPECTION AND POSSIBLE NECESSARY CORRECTIVE ACTION SHALL BE INCLUDED WITHIN THE ALLOWED CLOSURE PERIOD.

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REGISTERED PROFESSIONAL ENGINEER
72693PE
OREGON
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GREGORY W. VAUGHN
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NEWPORT MUNICIPAL AIRPORT
RUNWAY 16-34 REHABILITATION

LEGEND AND NOTES

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: **NPT008**

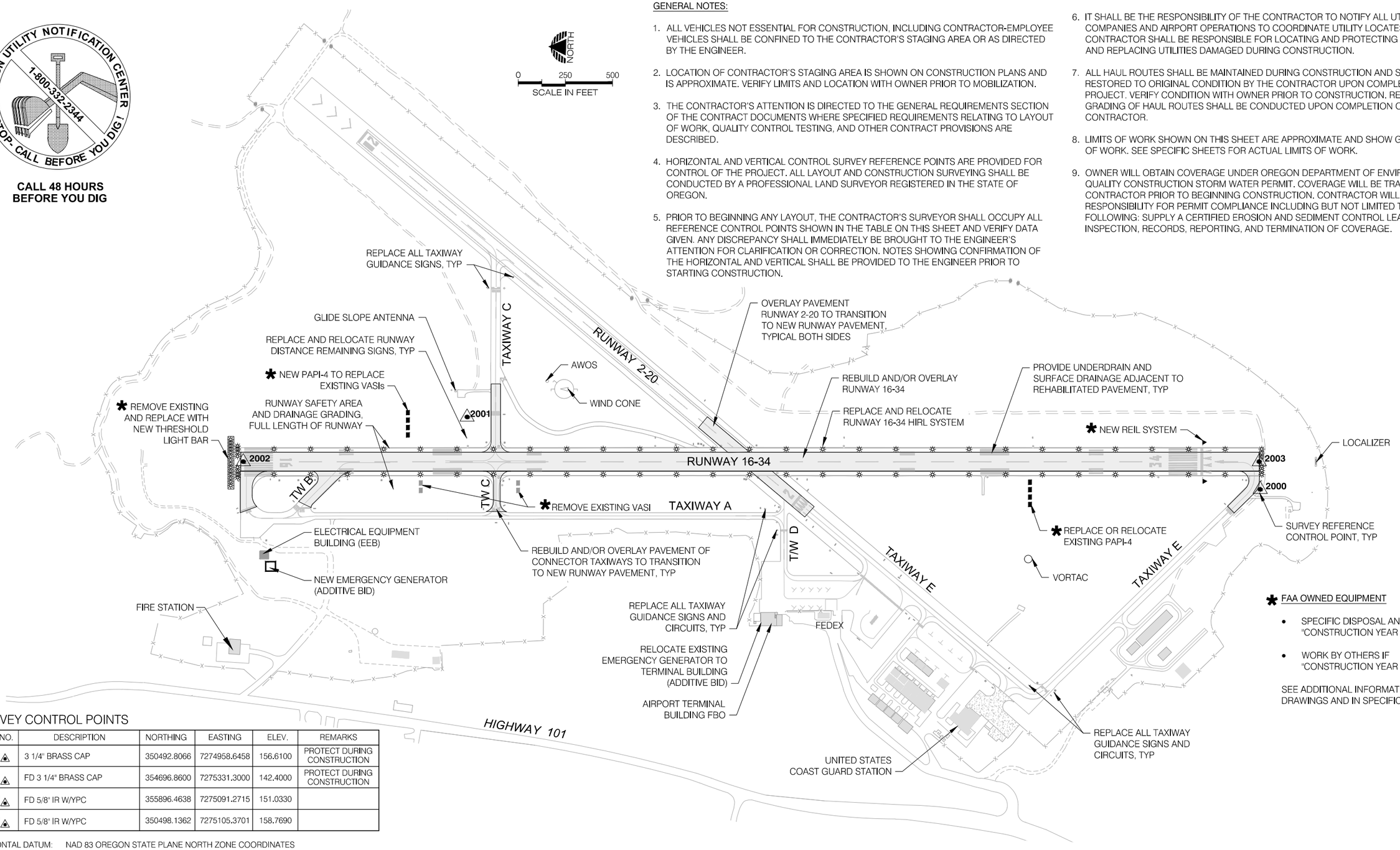
SHEET NO. **3** OF **108**

OREGON UTILITY NOTIFICATION CENTER
1-800-332-2344
STOP- CALL BEFORE YOU DIG !

CALL 48 HOURS BEFORE YOU DIG

GENERAL NOTES:

1. ALL VEHICLES NOT ESSENTIAL FOR CONSTRUCTION, INCLUDING CONTRACTOR-EMPLOYEE VEHICLES SHALL BE CONFINED TO THE CONTRACTOR'S STAGING AREA OR AS DIRECTED BY THE ENGINEER.
2. LOCATION OF CONTRACTOR'S STAGING AREA IS SHOWN ON CONSTRUCTION PLANS AND IS APPROXIMATE. VERIFY LIMITS AND LOCATION WITH OWNER PRIOR TO MOBILIZATION.
3. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE GENERAL REQUIREMENTS SECTION OF THE CONTRACT DOCUMENTS WHERE SPECIFIED REQUIREMENTS RELATING TO LAYOUT OF WORK, QUALITY CONTROL TESTING, AND OTHER CONTRACT PROVISIONS ARE DESCRIBED.
4. HORIZONTAL AND VERTICAL CONTROL SURVEY REFERENCE POINTS ARE PROVIDED FOR CONTROL OF THE PROJECT. ALL LAYOUT AND CONSTRUCTION SURVEYING SHALL BE CONDUCTED BY A PROFESSIONAL LAND SURVEYOR REGISTERED IN THE STATE OF OREGON.
5. PRIOR TO BEGINNING ANY LAYOUT, THE CONTRACTOR'S SURVEYOR SHALL OCCUPY ALL REFERENCE CONTROL POINTS SHOWN IN THE TABLE ON THIS SHEET AND VERIFY DATA GIVEN. ANY DISCREPANCY SHALL IMMEDIATELY BE BROUGHT TO THE ENGINEER'S ATTENTION FOR CLARIFICATION OR CORRECTION. NOTES SHOWING CONFIRMATION OF THE HORIZONTAL AND VERTICAL SHALL BE PROVIDED TO THE ENGINEER PRIOR TO STARTING CONSTRUCTION.
6. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO NOTIFY ALL UTILITY COMPANIES AND AIRPORT OPERATIONS TO COORDINATE UTILITY LOCATES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND PROTECTING ALL UTILITIES AND REPLACING UTILITIES DAMAGED DURING CONSTRUCTION.
7. ALL HAUL ROUTES SHALL BE MAINTAINED DURING CONSTRUCTION AND SHALL BE RESTORED TO ORIGINAL CONDITION BY THE CONTRACTOR UPON COMPLETION OF THE PROJECT. VERIFY CONDITION WITH OWNER PRIOR TO CONSTRUCTION. RESTORATION AND GRADING OF HAUL ROUTES SHALL BE CONDUCTED UPON COMPLETION OF USE BY THE CONTRACTOR.
8. LIMITS OF WORK SHOWN ON THIS SHEET ARE APPROXIMATE AND SHOW GENERAL AREAS OF WORK. SEE SPECIFIC SHEETS FOR ACTUAL LIMITS OF WORK.
9. OWNER WILL OBTAIN COVERAGE UNDER OREGON DEPARTMENT OF ENVIRONMENTAL QUALITY CONSTRUCTION STORM WATER PERMIT. COVERAGE WILL BE TRANSFERRED TO CONTRACTOR PRIOR TO BEGINNING CONSTRUCTION. CONTRACTOR WILL ASSUME ALL RESPONSIBILITY FOR PERMIT COMPLIANCE INCLUDING BUT NOT LIMITED TO THE FOLLOWING: SUPPLY A CERTIFIED EROSION AND SEDIMENT CONTROL LEAD, SITE INSPECTION, RECORDS, REPORTING, AND TERMINATION OF COVERAGE.



✱ FAA OWNED EQUIPMENT

- SPECIFIC DISPOSAL AND SALVAGE IF "CONSTRUCTION YEAR 2013"
- WORK BY OTHERS IF "CONSTRUCTION YEAR 2013"

SEE ADDITIONAL INFORMATION ON DRAWINGS AND IN SPECIFICATIONS.

SURVEY CONTROL POINTS

POINT NO.	DESCRIPTION	NORTHING	EASTING	ELEV.	REMARKS
2000	3 1/4" BRASS CAP	350492.8066	7274958.6458	156.6100	PROTECT DURING CONSTRUCTION
2001	FD 3 1/4" BRASS CAP	354696.8600	7275331.3000	142.4000	PROTECT DURING CONSTRUCTION
2002	FD 5/8" IR W/YPC	355896.4638	7275091.2715	151.0330	
2003	FD 5/8" IR W/YPC	350498.1362	7275105.3701	158.7690	

HORIZONTAL DATUM: NAD 83 OREGON STATE PLANE NORTH ZONE COORDINATES SHOWN ARE GROUND COORDINATES IN US SURVEY FEET
VERTICAL DATUM: NAVD 88

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5125 Southwest Hout Street
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NEWPORT MUNICIPAL AIRPORT
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SITE PLAN

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

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SHEET NO. **4** OF **108**

MATCH LINE THIS SHEET

NORTH

0 100 200 400
SCALE IN FEET

RUNWAY 2-20 RUNWAY SAFETY AREA (RSA)

RUNWAY 2-20 RUNWAY OBJECT FREE AREA (ROFA)

75' 250' 75' 250'

RUNWAY 2-20

TAXIWAY OBJECT FREE AREA (TOFA), TYPICAL ALL TAXIWAYS

TAXIWAY SAFETY AREA (TSA), TYPICAL ALL TAXIWAYS

TAXIWAY C

1 THRESHOLD LIGHT BAR

2 MALSR TOWER

2 GLIDE SLOPE ANTENNA

RUNWAY 16-34 RUNWAY OBJECT FREE AREA (ROFA)

RUNWAY 16-34 RUNWAY SAFETY AREA (RSA)

RUNWAY 16-34

16

34

250' 75' 250' 75'

300'

1 VASI

1 VASI

TW B

TAXIWAY A

TW D

300' TYP

1 PAPI

1 REIL

300'

2 WIND CONE

2 LOCALIZER

2 VORTAC

TAXIWAY E

CONTRACTOR'S STAGING AREA (SOUTH)

CONTRACTOR'S ALTERNATE STAGING AREA (SOUTH)

CONTRACTOR'S STAGING AREA (NORTH)

PROPOSED DISPOSAL AREAS FOR SELECT EXCESS MATERIAL, FINAL LOCATION TO BE DETERMINED BY OWNER OR ENGINEER, SEE PLANS AND SPECIFICATIONS

FOR CONTINUATION OF ACCESS ROUTE SEE SITE PLAN, SHEET 4

ELECTRICAL EQUIPMENT BUILDING (EEB)

FIRE STATION

CONTRACTOR'S ACCESS ROUTE, TYP

SECURITY FENCE, TYP

SECURITY FENCE

HIGHWAY 101

AUTOMATED VEHICLE GATE

TERMINAL BUILDING (FBO)

GENERAL AVIATION APRON

FEDEX

T-HANGARS

HANGAR AREA

UNITED STATES COAST GUARD

TOFA MODIFIED TO 79' WIDE IF CONTRACTOR'S ALTERNATE STAGING AREA (SOUTH) AND TEMPORARY ACCESS ROUTE IS REQUESTED FOR USE BY THE CONTRACTOR, AOA SHALL BE RESTRICTED TO AIRCRAFT USING T-HANGAR AREA ONLY

CONTRACTOR'S ALTERNATE ACCESS ROUTE, SEE DETAIL 1/9

HIGHWAY 101

LEGEND

- RSA — RUNWAY SAFETY AREA (150' WIDE*)
- ROFA — RUNWAY OBJECT FREE AREA (500' WIDE*)
- TSA — TAXIWAY SAFETY AREA (79' WIDE*)
- TOFA — TAXIWAY OBJECT FREE AREA (131' WIDE*)

* DIMENSION IS FULL WIDTH, HALF WIDTH SYMMETRICAL ABOUT CENTERLINE.

1 CONSTRUCTION YEAR 2013:
EQUIPMENT TO BE REMOVED, SALVAGED, AND DELIVERED TO THE FAA ONSITE LOCATION

CONSTRUCTION YEAR 2014:
EQUIPMENT TO BE REMOVED, REPLACED, OR RELOCATED BY OTHERS, DO NOT DISTURB UNTIL APPROVED

2 EQUIPMENT TO BE PROTECTED DURING CONSTRUCTION

NOTE:
THIS DRAWING IS INTENDED TO BE PRINTED IN COLOR. PRINTING IN BLACK AND WHITE MAY REDUCE READABILITY AND ALTER ENTITY DEFINITION OR REPRESENTATION.

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CONSTRUCTION SAFETY AND PHASING PLAN - AIRFIELD CRITICAL AREAS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: NPT008

SHEET NO. 5 OF 108

NOTE:
THE FOLLOWING REPRESENTS ONLY A PORTION OF THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP). REFER TO THE CONTRACT DOCUMENTS FOR THE FULL CSPP. PARAGRAPH NUMBERS BELOW REFER TO THE CSPP AND ADVISORY CIRCULAR 150/5370-2F.

DURING THE DURATION OF THE CONSTRUCTION PROJECT, THE CONTRACTOR MUST CONDUCT ACTIVITIES TO NOT VIOLATE ANY SAFETY STANDARDS CONTAINED IN AC 150/5370-2, OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION. IN ADDITION, THE FOLLOWING SAFETY AND OPERATIONAL PRACTICES SHALL BE OBSERVED:

205. COORDINATION. OPERATIONAL SAFETY SHALL BE A STANDING AGENDA ITEM DURING PROGRESS MEETINGS THROUGHOUT THE PROJECT.

206. PHASING. DRAWINGS SPECIFICALLY INDICATING OPERATIONAL SAFETY PROCEDURES AND METHODS IN AFFECTED AREAS HAVE BEEN DEVELOPED FOR EACH CONSTRUCTION PHASE. REFER TO THIS CONSTRUCTION SAFETY AND PHASING PLAN AND THE CONTRACT DRAWINGS.

207. AREAS AND OPERATIONS AFFECTED BY CONSTRUCTION ACTIVITY. THE CONSTRUCTION PHASING DRAWINGS HAVE BEEN DEVELOPED TO ALLOW RUNWAYS AND TAXIWAYS TO REMAIN IN USE TO THE MAXIMUM EXTENT POSSIBLE WITHOUT COMPROMISING SAFETY. THE FOLLOWING ITEMS ARE ADDRESSED: CLOSING OF RUNWAYS, TAXIWAYS AND APRONS, CLOSING OF ROUTES USED BY AIRPORT VEHICLES.

208. NAVIGATION AID (NAVAID) PROTECTION. THE AWOS CRITICAL AREA IS SHOWN ON THE PROJECT DRAWINGS. CONSTRUCTION ACTIVITY WITHIN THE CRITICAL AREAS OF NAVIGATION AIDS REQUIRES COORDINATION WITH FACILITY OWNER. THE CONTRACTOR SHALL GIVE THE AIRPORT OPERATOR OR RESIDENT ENGINEER ADVANCE NOTICE OF THIS ACTIVITY TO ALLOW COORDINATION WITH APPROPRIATE REPRESENTATIVES. STOCKPILING MATERIAL OR MOVEMENT/PARKING OF EQUIPMENT IS NOT ALLOWED IN THE CRITICAL AREA. APPROPRIATE NOTAMS MUST BE FILED FOR CONSTRUCTION ACTIVITIES.

209. CONTRACTOR ACCESS. THE PROJECT DRAWINGS SHOW THE AREAS TO WHICH THE CONTRACTOR HAS ACCESS, AND HOW CONTRACTOR PERSONNEL WILL ACCESS THOSE AREAS. ACCESS ROUTES ARE DESIGNED TO PREVENT INADVERTENT OR UNAUTHORIZED ENTRY ONTO THE AOA. CONTRACTOR EMPLOYEES SHALL PARK AND SERVICE ALL CONSTRUCTION VEHICLES IN AN AREA OUTSIDE THE OFA AND NEVER IN THE SAFETY AREA OF AN ACTIVE RUNWAY OR TAXIWAY. INACTIVE EQUIPMENT SHALL NOT BE PARKED ON A CLOSED TAXIWAY OR RUNWAY. THE CONTRACTOR SHALL NOT USE ANY ACCESS OR HAUL ROADS OTHER THAN THOSE APPROVED. ACCESS ROUTES USED BY CONTRACTOR VEHICLES SHALL BE CLEARLY MARKED TO PREVENT INADVERTENT ENTRY TO AREAS OPEN TO AIRPORT OPERATIONS. WHEN ANY VEHICLE, OTHER THAN ONE THAT HAS PRIOR APPROVAL FROM THE AIRPORT OWNER, MUST TRAVEL WITHIN ANY PORTIONS OF AN AOA, IT WILL BE ESCORTED AND PROPERLY IDENTIFIED AND LIGHTED. VEHICULAR TRAFFIC LOCATED IN OR CROSSING AN AOA MUST HAVE A WORKING TWO-WAY RADIO OR BE ESCORTED BY A VEHICLE WITH A RADIO. ALL DRIVERS SHALL CONFIRM THAT NO AIRCRAFT IS APPROACHING THE VEHICLE POSITION. CONSTRUCTION PERSONNEL MAY OPERATE IN AN AOA WITHOUT TWO-WAY RADIO COMMUNICATION PROVIDED A NOTAM IS ISSUED CLOSING THE AREA AND THE AREA IS PROPERLY MARKED AND BARRICADED TO PREVENT INCURSIONS. CONTRACTOR SHALL MONITOR THE COMMON TRAFFIC ADVISORY FREQUENCY (CTAF) WHILE WORKING IN CLOSED AREAS. DRIVERS SHALL CONFIRM BY PERSONAL OBSERVATION THAT NO AIRCRAFT IS APPROACHING THEIR POSITION (EITHER IN THE AIR OR ON THE GROUND) WHEN CROSSING A RUNWAY, TAXIWAY, OR OTHER AREA OPEN TO AIRPORT OPERATIONS. IN ADDITION, IT IS THE RESPONSIBILITY OF THE ESCORT VEHICLE DRIVER TO VERIFY THE MOVEMENT/POSITION OF ALL ESCORTED VEHICLES AT ANY GIVEN TIME. CONSTRUCTION CONTRACTOR PERSONNEL ENGAGED IN ACTIVITIES INVOLVING UNESCORTED OPERATION ON AIRCRAFT MOVEMENT AREAS MUST OBSERVE THE PROPER PROCEDURES FOR COMMUNICATIONS, INCLUDING USING THE APPROPRIATE RADIO FREQUENCY (123.075 MHZ). WHEN OPERATING VEHICLES ON OR NEAR OPEN RUNWAYS OR TAXIWAYS, CONSTRUCTION PERSONNEL MUST ANNOUNCE THEIR INTENTIONS AND CONTINUE TO MONITOR AIRCRAFT OPERATIONS. PROCEDURES SHALL BE IN PLACE TO ENSURE THAT ONLY AUTHORIZED PERSONS AND VEHICLES HAVE ACCESS TO THE AOA AND TO PROHIBIT "PIGGYBACKING" BEHIND ANOTHER PERSON OR VEHICLE.

210. WILDLIFE MANAGEMENT. THE CONTRACTOR SHALL CAREFULLY CONTROL AND CONTINUOUSLY REMOVE WASTE OR LOOSE MATERIALS THAT MIGHT ATTRACT WILDLIFE. CONTRACTOR PERSONNEL SHALL IMMEDIATELY NOTIFY THE AIRPORT OPERATOR OF WILDLIFE SIGHTINGS.

211. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT. THE CONTRACTOR SHALL NOT LEAVE OR PLACE FOD ON OR NEAR ACTIVE AIRCRAFT MOVEMENT AREAS. MATERIALS TRACKED ONTO THESE AREAS MUST BE REMOVED IMMEDIATELY. MATERIALS CAPABLE OF CREATING FOD SHALL BE CONTINUOUSLY REMOVED DURING THE CONSTRUCTION PROJECT.

212. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT. THE CONTRACTORS SHALL BE PREPARED TO EXPEDITIOUSLY CONTAIN AND CLEAN-UP SPILLS RESULTING FROM FUEL OR HYDRAULIC FLUID LEAKS

213. NOTIFICATION OF CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE AIRPORT OPERATIONS MANAGER OR ENGINEER OF ANY CONDITIONS ADVERSELY AFFECTING THE OPERATIONAL SAFETY OF THE AIRPORT. BEFORE BEGINNING ANY CONSTRUCTION ACTIVITY WHICH MAY IMPACT THE NORMAL OPERATIONS AT THE AIRPORT THE CONTRACTOR MUST ENSURE THAT THE ACTIVITY HAS BEEN REPORTED USING THE FAA'S NOTICE TO AIRMEN (NOTAM) SYSTEM. UPON COMPLETION OF WORK AND RETURN OF AREAS TO STANDARD CONDITIONS, THE CONTRACTOR MUST VERIFY THE CANCELLATION OF ALL APPLICABLE NOTAMS. ONLY THE AIRPORT OPERATOR MAY INITIATE OR CANCEL NOTAMS, AND IS THE ONLY ENTITY THAT CAN CLOSE OR OPEN A RUNWAY. ALL COMMUNICATION WITH THE FAA WILL BE ACCOMPLISHED BY THE AIRPORT OPERATIONS MANAGER.

214. INSPECTION REQUIREMENTS. INSPECTIONS WILL BE CONDUCTED DAILY, OR MORE FREQUENTLY IF NECESSARY, TO ENSURE CONFORMANCE WITH THIS CSPP. A CONSTRUCTION PROGRESS DAILY SAFETY (CPDS) CHECKLIST SHALL BE USED.

215. UNDERGROUND UTILITIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CALLING THE UTILITY NOTIFICATION CENTER TO LOCATE PUBLIC UTILITIES AND FOR ANY ADDITIONAL UTILITY LOCATES INCLUDING HIRING A PRIVATE UTILITY LOCATE SERVICE IF REQUIRED.

216. PENALTIES. CONTRACTOR PERSONNEL WHO VIOLATE SAFETY REQUIREMENTS MAY BE REMOVED FROM THE PROJECT AT THE DISCRETION OF THE OWNER.

218. RUNWAY AND TAXIWAY VISUAL AIDS. THE CONTRACTOR SHALL INSURE THAT AREAS WHERE AIRCRAFT WILL BE OPERATING ARE CLEARLY AND VISIBLY SEPARATED FROM CONSTRUCTION AREAS.

220. HAZARD MARKING, LIGHTING AND SIGNING. HAZARDOUS AREAS IN THE AOA, (INCLUDING CONSTRUCTION AREAS NORMALLY ACCESSIBLE TO AIRCRAFT), OPEN MANHOLES, AREAS UNDER REPAIR, STOCKPILED MATERIAL, WASTE AREAS, AND AREAS SUBJECT TO JET BLAST, SHALL BE MARKED WITH BARRICADES. DURING PERIODS OF LOW VISIBILITY AND AT NIGHT, RED FLASHING LIGHTS SHALL BE OPERATIONAL ON THE BARRICADES. THE HAZARDOUS AREA MARKING AND LIGHTING SHALL BE FURNISHED AND MAINTAINED BY THE CONTRACTOR. LIGHTING SHALL BE CHECKED FOR PROPER OPERATION AT LEAST ONCE PER DAY, PREFERABLY AT DUSK.

221. PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS. RUNWAY AND TAXIWAY SAFETY AREAS (RSA) (TSA), OBSTACLE FREE ZONES (OFZ), RUNWAY AND TAXIWAY OBJECT FREE AREAS (ROFA) (TOFA), AND APPROACH AND DEPARTURE SURFACES SHALL BE PROTECTED AT ALL TIMES BY THE CONTRACTOR. NO CONSTRUCTION MAY OCCUR WITHIN THE RSA OR TOFA WHILE THE RUNWAY OR TAXIWAY IS OPEN FOR AIRCRAFT OPERATIONS. OPEN TRENCHES OR EXCAVATIONS ARE NOT PERMITTED WITHIN THE RSA OR TSA WHILE THE RUNWAY OR TAXIWAY IS OPEN. CONSTRUCTION, INCLUDING EXCAVATIONS, MAY BE PERMITTED IN THE RUNWAY OFA, HOWEVER, EQUIPMENT MUST BE REMOVED WHEN NOT IN USE, AND MATERIAL SHALL NOT BE STOCKPILED IN THE RUNWAY OFA. NO CONSTRUCTION MAY OCCUR WITHIN THE TAXIWAY OBJECT FREE AREA WHILE THE TAXIWAY IS OPEN FOR AIRCRAFT OPERATIONS. WORK UP TO THE EDGE OF THE RSA MAY BE ALLOWED UPON APPROVAL OF THE OWNER AND FAA AND AN APPROPRIATE NOTAM IS ISSUED.

222. OTHER LIMITATIONS ON CONSTRUCTION: THE AIRPORT SHALL HAVE THE AUTHORITY TO SUSPEND THE WORK WHOLLY, OR IN PART, FOR SUCH PERIOD AS NECESSARY, DUE TO CONDITIONS CONSIDERED UNFAVORABLE FOR THE PROSECUTION OF THE WORK, OR DUE TO THE FAILURE OF THE CONTRACTOR TO CARRY OUT ORDERS GIVEN OR PERFORM PROVISIONS OF THE CONTRACT. ANY VEHICLE OPERATING IN THE AOA DURING HOURS OF DARKNESS OR REDUCED VISIBILITY MUST BE EQUIPPED WITH AN AMBER FLASHING BEACON. WHEN USED DURING PERIODS OF DARKNESS, ALL BARRICADES, WARNING SIGNS, AND HAZARD MARKINGS SHALL BE SUITABLY ILLUMINATED.

CONSTRUCTION TIME AND SCHEDULE:

1. THIS PROJECT WILL HAVE TWO CONSTRUCTION PERIOD OPTIONS.
   a. OPTION ONE, "CONSTRUCTION YEAR 2013": CONSTRUCTION BEGINS THIS YEAR AS SOON AS POSSIBLE AFTER BID OPENING. CONSTRUCTION WORK PERIOD (CONTRACT PERIOD):140 CALENDAR DAYS.
   b. OPTION TWO, "CONSTRUCTION YEAR 2014": WORK ACCOMPLISHED DURING CONSTRUCTION SEASON 2014 SHALL NOT EXTEND BEYOND OCTOBER 15, 2014. CONSTRUCTION WORK PERIOD (CONTRACT PERIOD): 120 CALENDAR DAYS.
2. THE PROJECT IS DIVIDED INTO TWO CONSTRUCTION PHASES, AS SHOWN ON SHEETS 7 AND 8. CONSTRUCTION PHASE 1: WORK WILL BE ACCOMPLISHED WHILE RUNWAY 16-34 IS CLOSED AND RUNWAY 2-20 REMAINS OPERATIONAL. CONSTRUCTION PHASE 2: RUNWAY 16-34 AND RUNWAY 2-20 ARE CLOSED FOR A MAXIMUM OF 10 CONSECUTIVE 24-HOUR PERIODS TO ACCOMPLISH MID-FIELD IMPROVEMENTS. CLOSING BOTH RUNWAYS WILL CONSTITUTE THE ENTIRE AIRPORT CLOSURE. SEE ADDITIONAL REQUIREMENTS FOR AIRPORT CLOSURE.
3. THE TOTAL CONSTRUCTION PERIOD (CONTRACT PERIOD) IS ESTABLISHED AT A MAXIMUM OF 140 OR 120 CALENDAR DAYS, DEPENDING ON SELECTED CONSTRUCTION YEAR OPTION. CONTRACTOR SHALL DEVELOP A WORK SCHEDULE THAT MINIMIZES DISRUPTION TO THE AIRPORT.

CONTRACT PERIOD, MILESTONE DATES, AND LIQUIDATED DAMAGES

1. BOTH CONSTRUCTION YEAR 2013 OPTION AND CONSTRUCTION YEAR 2014 OPTION HAVE MILESTONE DATES AND/OR TIME PERIOD REQUIREMENTS. LIQUIDATED DAMAGES ASSOCIATED WITH MILESTONE/TIME PERIOD REQUIREMENTS ARE DESCRIBED BELOW:
   a. CONSTRUCTION YEAR 2013 OPTION.
      i. IT IS ANTICIPATED CONSTRUCTION WILL BEGIN MID TO LATE AUGUST, 2013. ALL WORK MUST BE COMPLETED WITHIN 140 CALENDAR DAYS FROM THE NTP WITH CONSTRUCTION. THIS IS CONSIDERED THE "CONTRACT PERIOD." NO CONTRACT TIME EXTENSIONS WILL BE PROVIDED FOR POOR WEATHER OR WET CONDITIONS AS SUCH CONDITIONS ARE CONSIDERED NORMAL COASTAL WEATHER DURING THAT TIME OF YEAR. IF THE PROJECT IS NOT SUBSTANTIALLY COMPLETE WITHIN 140 CALENDAR DAYS, THEN A $1,500 PER DAY LIQUIDATED DAMAGE WILL BE ASSESSED FOR EACH DAY BEYOND THE CONTRACT PERIOD.
      ii. IF CONTRACTOR FAILS TO REOPEN RUNWAY 2-20 OR RUNWAY 16-34 AFTER THE ALLOWED 10 CONSECUTIVE 24-HOUR PERIODS DURING CONSTRUCTION PHASE 2, A LIQUIDATED DAMAGE OF $5,000 PER DAY WILL BE ASSESSED FOR EACH DAY, OR PORTION OF DAY, THAT AT LEAST ONE OF THE RUNWAYS IS NO OPERATIONAL.
      iii. IT IS IMPERATIVE THAT RECONSTRUCTION OF RUNWAY 16-34 PAVEMENT AREA BE ACCOMPLISHED EARLY DURING THE PROJECT. THEREFORE, THE RUNWAY 16-34 REBUILD MUST BE CONSTRUCTED WITH AT LEAST THE FIRST LIFT OF BITUMINOUS SURFACE COURSE (COVERING ALL BASE COURSE MATERIALS) BY NO LATER THAN SEPTEMBER 30, 2013. FAILURE TO PROVIDE THIS REQUIREMENT WILL RESULT IN LIQUIDATED DAMAGES OF $1,500 PER DAY UNTIL ACCOMPLISHED.
      iv. THE LIQUIDATED DAMAGES WILL BE ASSESSED ACCUMULATIVE (IF APPLICABLE) UNTIL SATISFACTORY CONDITIONS ARE MET.
   b. CONSTRUCTION YEAR 2014 OPTION.
      i. IF SELECTED IT SHALL BE UNDERSTOOD THAT THE CONTRACTOR'S BID PRICES WILL REMAIN THE SAME. ALL WORK MUST BE COMPLETED WITHIN 120 CALENDAR DAYS FROM THE NTP WITH CONSTRUCTION. IF THE PROJECT IS NOT SUBSTANTIALLY COMPLETE WITHIN 120 CALENDAR DAYS, THEN A $1,500 PER DAY LIQUIDATED DAMAGE WILL BE ASSESSED FOR EACH DAY BEYOND THE CONTRACT PERIOD.
      ii. IF THE CONTRACTOR FAILS TO REOPEN RUNWAY 2-20 OR RUNWAY 16-34 AFTER THE ALLOWED 10 CONSECUTIVE 24-HOUR PERIODS DURING CONSTRUCTION PHASE 2, A LIQUIDATED DAMAGE OF $5,000 PER DAY WILL BE ASSESSED FOR EACH DAY, OR PORTION OF A DAY, THAT AT LEAST ONE OF THE RUNWAYS IS NOT OPERATIONAL.
      iii. THE LIQUIDATED DAMAGES WILL BE ASSESSED ACCUMULATIVE (IF APPLICABLE) UNTIL SATISFACTORY CONDITIONS ARE MET.

OTHER ACTIVITY AND REQUIREMENTS IMPACTING THIS PROJECT:

1. CONSTRUCTION YEAR 2013 OPTION: SOME NAVAIDS AND EQUIPMENT OWNED AND MAINTAINED BY FAA WILL BE REMOVED, SALVAGED AND STORED ON AIRPORT PROPERTY BY THIS PROJECT IF CONSTRUCTION IN YEAR 2013 IS SELECTED. TASKS ASSOCIATED WITH THIS WORK ARE INDICATED ON THE ELECTRICAL DEMOLITION PLANS AND ELSEWHERE IN THIS DOCUMENT, OR AS OTHERWISE DIRECTED BY THE ENGINEER. WORK SHALL INCLUDE, BUT NOT BE LIMITED TO, REMOVAL OF THRESHOLD BAR AT END OF RUNWAY 16; REMOVAL OF VASI SYSTEM ON WEST SIDE OF RUNWAY 16, REMOVAL OF THE EXISTING PAPI SYSTEM ON THE WEST SIDE OF RUNWAY 34; REMOVAL OF THE REILS ON RUNWAY 34 AND VARIOUS ASSOCIATED POWER AND COMMUNICATION CABLES, CONDUITS, HANDHOLES, JUNCTION CANS AND MISCELLANEOUS EQUIPMENT. PAYMENT WILL BE MADE UNDER CONSTRUCTION YEAR 2013 OPTION BASE BID SCHEDULE, BIT ITEM NO.59-FAA EQUIPMENT DEMOLITION.
   ADDITIONAL INFORMATION, PROVIDED BY FAA, RELATED TO THE FAA-OWNED NAVAID WORK IS INCLUDED IN THE ORPIN DIRECTORY AS A SUPPLEMENT ENTITLED "FAA NAVIGATION AIDS PLAN AND SPECS-90%." THE ADDITIONAL INFORMATION INCLUDES PRELIMINARY DRAWINGS AND SPECIFICATIONS (NOT FOR CONSTRUCTION) THAT WERE BEING DEVELOPED BY FAA AT THE TIME THIS PROJECT ADVERTISED. VETTED AND STAMPED DRAWINGS WILL BE PROVIDED BY AIRPORT SERVICES PRIOR TO DEMOLITION.
2. CONSTRUCTION YEAR 2014 OPTION: NAVAIDS AND EQUIPMENT OWNED AND MAINTAINED BY FAA WILL BE REMOVED, REPLACED, AND/OR RELOCATED BY OTHERS UNDER SEPARATE CONTRACT DURING THIS PROJECT PERIOD. CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH "WORK BY OTHERS". WORK BY OTHERS SHALL INCLUDE, BUT NOT BE LIMITED TO, REMOVAL AND REPLACEMENT OF THRESHOLD BAR AT END OF RUNWAY 16; REMOVAL OF EXISTING VASI SYSTEM ON WEST SIDE OF RUNWAY 16 AND REPLACING WITH NEW PAPI SYSTEM ON EAST SIDE; RELOCATING OR REPLACING THE PAPI SYSTEM ON THE WEST SIDE OF RUNWAY 34; REPLACING THE REILS ON RUNWAY 34 AND ASSOCIATED POWER INSTALLATION, POWER ROUTING, ASSOCIATED ELECTRICAL EQUIPMENT BUILDING IMPROVEMENTS AND APPLICABLE CERTIFICATIONS.
3. OTHER NAVAIDS AND EQUIPMENT ON THE AIRPORT POSSIBLY INFLUENCED BY CONSTRUCTION ACTIVITIES DURING EITHER CONSTRUCTION YEAR OPTION SELECTED SUCH AS THE ILS ANTENNA, VORTAC AND LOCALIZER SHALL BE PROTECTED AT ALL TIMES. THIS EQUIPMENT WILL BE TURNED OFF OR DECOMMISSIONED BY FAA TECHNICIANS AS APPROPRIATE WHEN SHUTTING DOWN THE INSTRUMENT APPROACH LANDING SYSTEM.

PRECISION APPROACH ENGINEERING
5125 Southwest Hout Street
Corvallis, OR 97333
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REGISTERED PROFESSIONAL ENGINEER
7369PE
OREGON
JUNE 14, 2007
GEOFF W. VAUGHN
EXPIRES 6/30/2014

REVISIONS: DATE APPD.

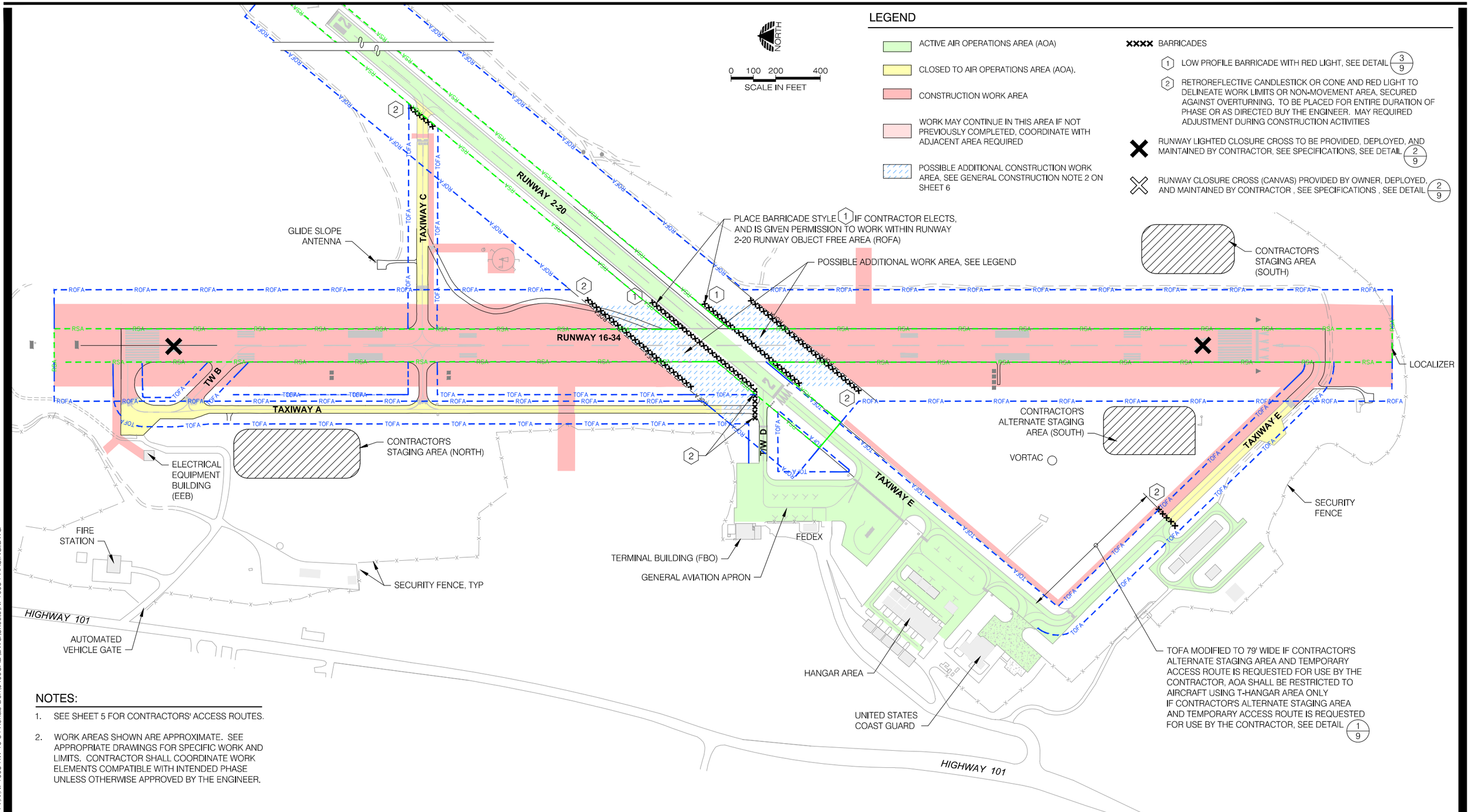
DATE: JUNE 2013
DESIGN: TLK
DRAWN: GPG
CHECKED: GWV
REVISION NUMBER: 0
SCALE: AS SHOWN

NEWPORT MUNICIPAL AIRPORT
RUNWAY 16-34 REHABILITATION

CONSTRUCTION SAFETY AND PHASING PLAN - NOTES

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: NPT008
SHEET NO. 6
OF 108



NOTES:

1. SEE SHEET 5 FOR CONTRACTORS' ACCESS ROUTES.
2. WORK AREAS SHOWN ARE APPROXIMATE. SEE APPROPRIATE DRAWINGS FOR SPECIFIC WORK AND LIMITS. CONTRACTOR SHALL COORDINATE WORK ELEMENTS COMPATIBLE WITH INTENDED PHASE UNLESS OTHERWISE APPROVED BY THE ENGINEER.

NOTE:
THIS DRAWING IS INTENDED TO BE PRINTED IN COLOR. PRINTING IN BLACK AND WHITE MAY REDUCE READABILITY AND ALTER ENTITY DEFINITION OR REPRESENTATION.

PRECISION APPROACH ENGINEERING
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REGISTERED PROFESSIONAL ENGINEER 73693PE OREGON JUNE 14, 2007 GEOFF W. VAUGHN EXPIRES 6/30/2014

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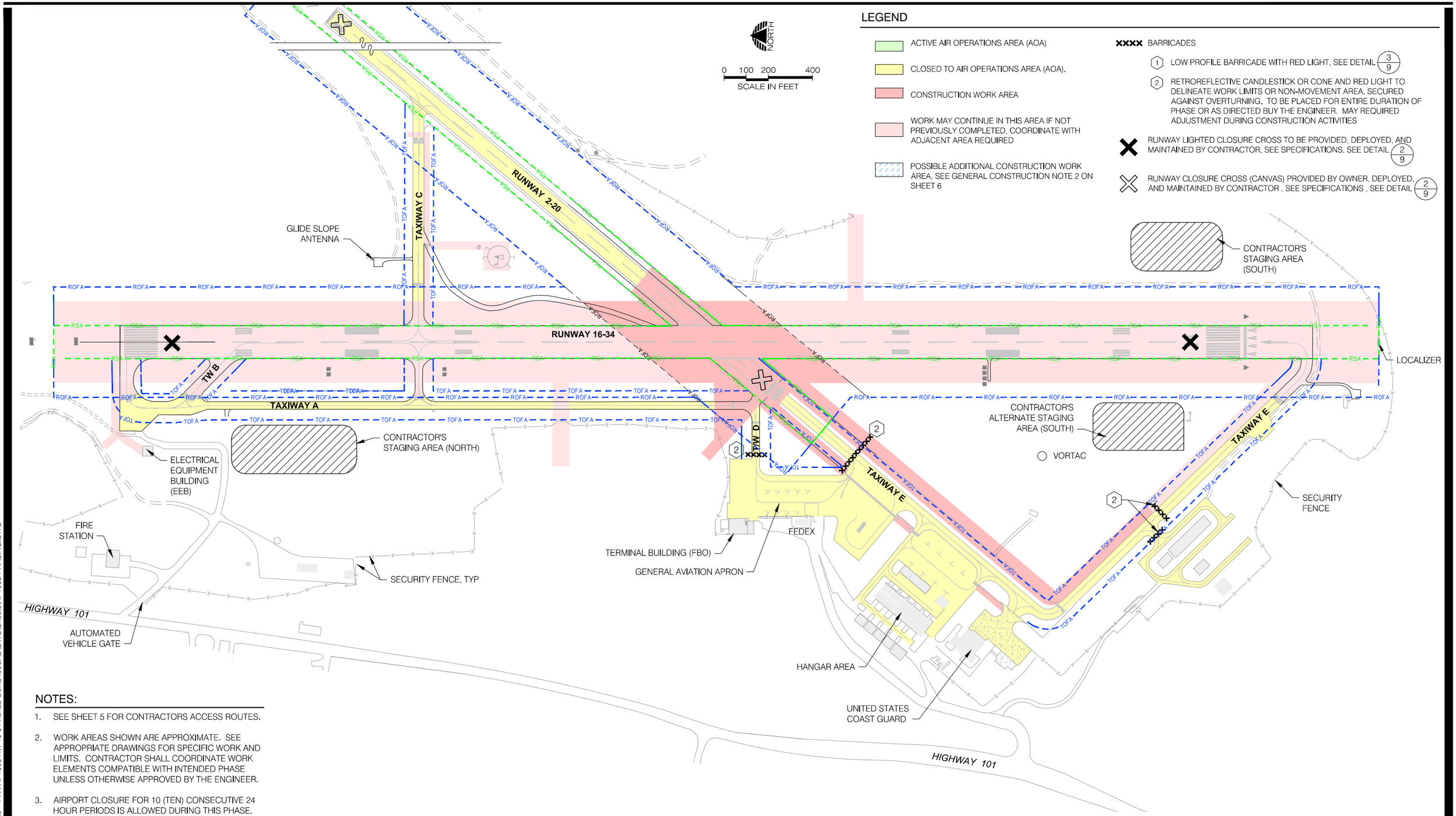
NEWPORT MUNICIPAL AIRPORT
RUNWAY 16-34 REHABILITATION

CONSTRUCTION SAFETY AND PHASING PLAN - CONSTRUCTION PHASE 1

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: NPT008

SHEET NO. 7 OF 108



NOTES:

1. SEE SHEET 5 FOR CONTRACTORS ACCESS ROUTES.
2. WORK AREAS SHOWN ARE APPROXIMATE. SEE APPROPRIATE DRAWINGS FOR SPECIFIC WORK AND LIMITS. CONTRACTOR SHALL COORDINATE WORK ELEMENTS COMPATIBLE WITH INTENDED PHASE UNLESS OTHERWISE APPROVED BY THE ENGINEER.
3. AIRPORT CLOSURE FOR 10 (TEN) CONSECUTIVE 24 HOUR PERIODS IS ALLOWED DURING THIS PHASE. SEE CONSTRUCTION TIME AND SCHEDULE NOTES ON SHEET 6.

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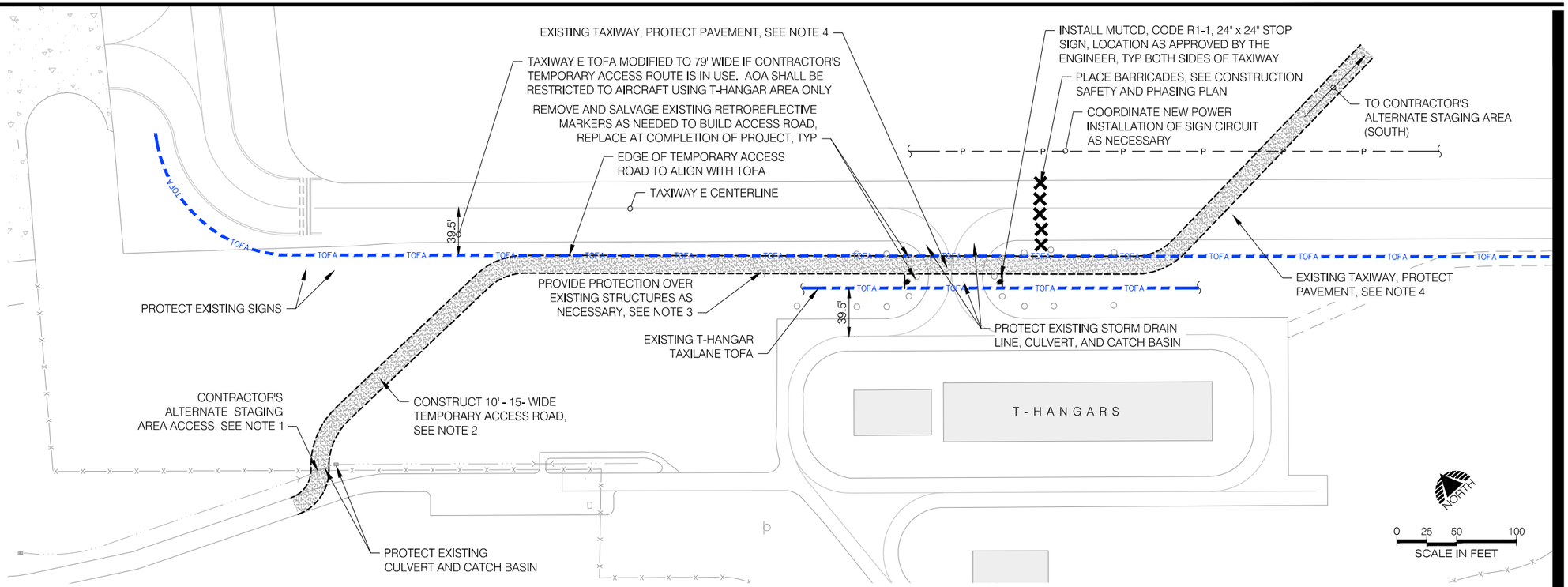
NEWPORT MUNICIPAL AIRPORT
RUNWAY 16-34 REHABILITATION
CONSTRUCTION SAFETY AND PHASING PLAN - CONSTRUCTION PHASE 2
PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: NPT008

SHEET NO. 8 OF 108

CONTRACTOR'S ALTERNATE STAGING AREA ACCESS NOTES:

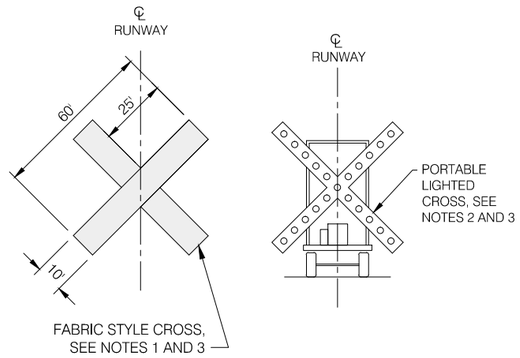
1. IF DESIRED, CONTRACTOR MAY ESTABLISH TEMPORARY ACCESS TO CONTRACTOR'S ALTERNATE STAGING AREA THROUGH EXISTING FENCE. CAREFULLY REMOVE EXISTING FENCE AS NECESSARY AND REPLACE WITH NEW 7' HIGH CHAIN LINK GATE CAPABLE OF BEING LOCKED IN ORDER TO MAINTAIN SECURITY. ANY TIME GATE IS LEFT OPEN CONTRACTOR SHALL PROVIDE GUARD PERSON AT ENTRANCE IN ORDER TO MAINTAIN SECURITY.
2. CONTRACTOR SHALL BUILD TEMPORARY ROAD CAPABLE OF SUPPORTING CONSTRUCTION EQUIPMENT WITH CRUSHED AGGREGATE OR OTHER ACCEPTABLE MATERIALS APPROVED BY THE ENGINEER.
3. IF EXISTING UTILITY OR DRAINAGE STRUCTURES ARE ENCOUNTERED IN TEMPORARY ROAD ALIGNMENT PROVIDE 1-INCH THICK STEEL PLATE OVER STRUCTURE, OR AS NECESSARY, FOR ADEQUATE PROTECTION FROM VEHICLES OR EQUIPMENT.
4. PROTECT EXISTING PAVEMENTS. PLACE PROTECTIVE MEMBRANE ON EXISTING PAVEMENT AND 1-INCH THICK STEEL PLATES WHERE TRAFFIC WILL BE CROSSING OR AS OTHERWISE DIRECTED BY THE ENGINEER.
5. CONTRACTOR TO RETURN ALL AFFECTED AREAS BACK TO EXISTING CONDITION AT COMPLETION OF PROJECT. NO DIRECT PAYMENT, INCIDENTAL TO MOBILIZATION/DEMOBLILIZATION BID ITEM.



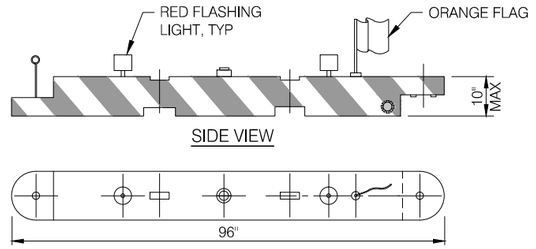
CONTRACTOR'S TEMPORARY ACCESS TO ALTERNATE STAGING AREA PLAN — 1 / 9
NTS

RUNWAY CLOSURE CROSS NOTES:

1. TWO FAA APPROVED FABRIC STYLE TEMPORARY CLOSURE CROSSES WILL BE PROVIDED BY THE OWNER FOR USE BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACEMENT AND MAINTAINING THE CONDITION OF THE CROSSES UNTIL THEY ARE RETURNED TO THE OWNER UPON COMPLETION OF THE PROJECT. CROSSES MUST BE PROPERLY CONFIGURED AND SECURED TO PREVENT MOVEMENT BY PROP WASH, JET BLAST, OR OTHER WIND CURRENTS AS REQUIRED OR DIRECTED BY THE ENGINEER. DAMAGE OR OTHER AVOIDABLE DETERIORATION TO THE CROSSES WHILE UNDER THE RESPONSIBILITY OF THE CONTRACTOR SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE OWNER. PRIOR TO TURNING OVER TO THE OWNER, THE CONTRACTOR SHALL THOROUGHLY CLEAN AND DRY THE CROSSES. SEE SPECIFICATIONS FOR ADDITIONAL INFORMATION AND REQUIREMENTS.
2. TWO NEW PORTABLE TRAILER MOUNTED, LIGHTED CLOSURE CROSSES (MARKERS) WILL BE PROVIDED BY THE CONTRACTOR WHEN CLOSURE OF RUNWAY 16-34 IS REQUIRED. PLACEMENT AND PROTECTION IS THE SAME AS REFERENCED IN NOTE 3, BELOW. SEE SPECIFICATIONS FOR ADDITIONAL INFORMATION AND REQUIREMENTS.
3. PLACE CLOSURE CROSSES AT THE RUNWAY NUMERALS OR AS INDICATED ON THE DRAWINGS ANYTIME RUNWAY 16-34 IS CLOSED TO OPERATIONS. PLACEMENT OF FABRIC CROSSES IS INTENDED FOR CLOSURE OF RUNWAY 2-20. THE ENGINEER WILL DESIGNATE ALTERNATE LOCATIONS FOR PLACEMENT OF THE CROSSES WHEN PLACEMENT OVER THE NUMERALS CONFLICTS WITH CONSTRUCTION ACTIVITIES.



RUNWAY CLOSURE CROSS DETAIL — 2 / 9
NTS



NOTES:

1. PROVIDE BARRICADE CAPABLE OF BEING FILLED WITH WATER OR SAND. IF ALTERNATE METHOD OF ANCHORING IS USED IT SHALL NOT CAUSE DAMAGE TO PAVEMENT.
2. BARRICADE TO BE CAPABLE OF BEING DEPLOYED BY ONE PERSON WHEN EMPTY.
3. LINK BARRICADES TOGETHER AS RECOMMENDED BY MANUFACTURER. PLACE BARRICADES CONTINUOUSLY ACROSS AREAS TO BE CLOSED OR AS DIRECTED BY ENGINEER.

PORTABLE PLASTIC BARRICADE DETAIL — 3 / 9
NTS

PRECISION APPROACH ENGINEERING
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REGISTERED PROFESSIONAL ENGINEER — 72693PE — OREGON — JUNE 14, 2007 — GEOFF W. VAUGHN — EXPIRES 6/30/2014

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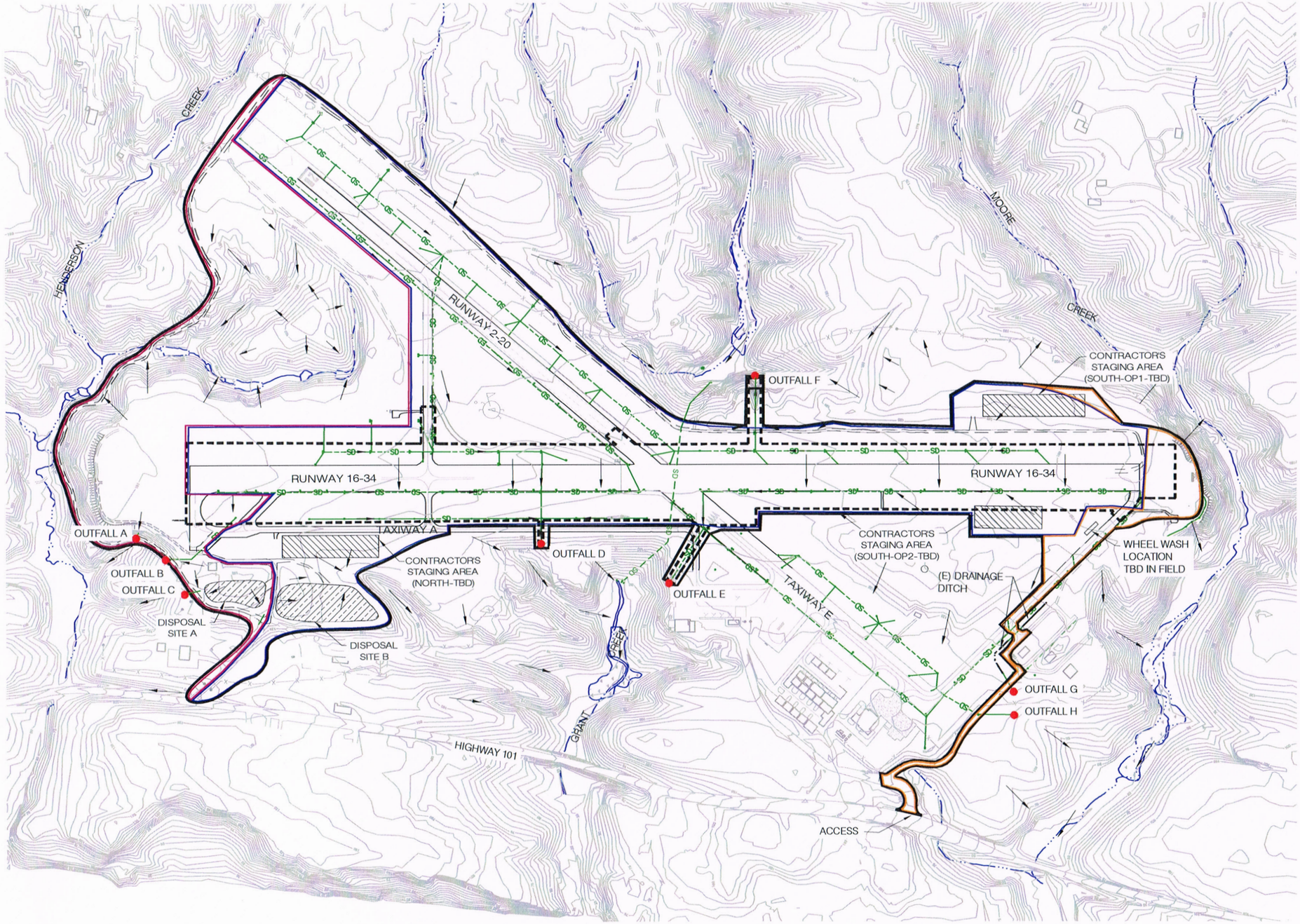
NEWPORT MUNICIPAL AIRPORT
RUNWAY 16-34 REHABILITATION

CONSTRUCTION SAFETY AND PHASING PLAN - DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: NPT008

SHEET NO. 9 OF 108



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0 150 300 600
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LEGEND

- PROJECT IMPACT BOUNDARY
- CONSTRUCTION BOUNDARY
- DIRECTION OF FLOW
- MOORE CREEK DRAINAGE AREA
- GRANT CREEK DRAINAGE AREA
- HENDERSON CREEK DRAINAGE AREA
- TRAVEL WAY
- DRAINAGE OUTFALL
- EXISTING STORM DRAIN
- EXISTING CREEK

NOTES:

1. KEEP TRAFFIC ON DESIGNATED TRAVEL WAYS AND AVOID IMPACTS TO VEGETATED AREAS.
2. KEEP PAVED AREAS CLEAN AND SWEEP REGULARLY.
3. DISPOSAL AREAS TO BE COMPACTED AND SEEDED ACCORDING TO SPECIFICATIONS.
4. STOCKPILES TO BE COVERED OR STABILIZED AS NECESSARY DURING FORECASTED STORM EVENTS (>0.25 INCHES).
5. DISPOSAL SITE TO BE KEPT ORGANIZED AND SURROUNDING AREA CLEAN.
6. DIVERT UNCONTAMINATED WATER AROUND DISPOSAL SITE AND STOCKPILES.
7. INSPECT DISPOSAL SITE AND STOCKPILES REGULARLY AND REPAIR/REPLACE COVERS AND PERIMETER CONTROLS AS NEEDED.
8. IMPLEMENT DUST CONTROL PRACTICES SUCH AS WATER SPRAYING AS REQUIRED TO PREVENT WIND EROSION.
9. ALL DISTURBED AREAS TO BE SEEDED WITH SPECIFIED SEED MIX POST-CONSTRUCTION. SEE SPECIFICATIONS.
10. EXISTING CATCH BASINS AND INLETS TO BE PROTECTED PRIOR TO DEMOLITION. NEWLY INSTALLED CATCH BASINS AND INLETS TO BE PROTECTED UNTIL VEGETATION IS ESTABLISHED.
11. MONITOR STORM SYSTEM INLETS DAILY AND MAINTAIN ACCORDING TO SPECIFICATIONS AND DETAIL SHEET
12. MONITOR ALL OUTFALLS. IF DISCHARGE APPEARS TURBID, CONSULT ENGINEER AND IMPLEMENT ADDITIONAL BMPS AS REQUIRED.
13. PRE- AND POST-CONSTRUCTION DRAINAGE PATTERNS ARE GENERALLY THE SAME, WITH THE EXCEPTION OF RUNWAY 16-34 CONVERTING TO A CROWN SECTION FROM A SHED SECTION.
14. IMPERVIOUS AREAS AT THE CONSTRUCTION SITE ARE SHOWN. IMPERVIOUS AREA WILL DECREASE POST-CONSTRUCTION DUE TO THE REDUCTION IN RUNWAY WIDTH.

REGISTERED PROFESSIONAL ENGINEER
11,146
OREGON
JULY 17, 1981
RONALD F. STILLMAKER
EXPIRES 06/30/14

PRECISION APPROACH ENGINEERING
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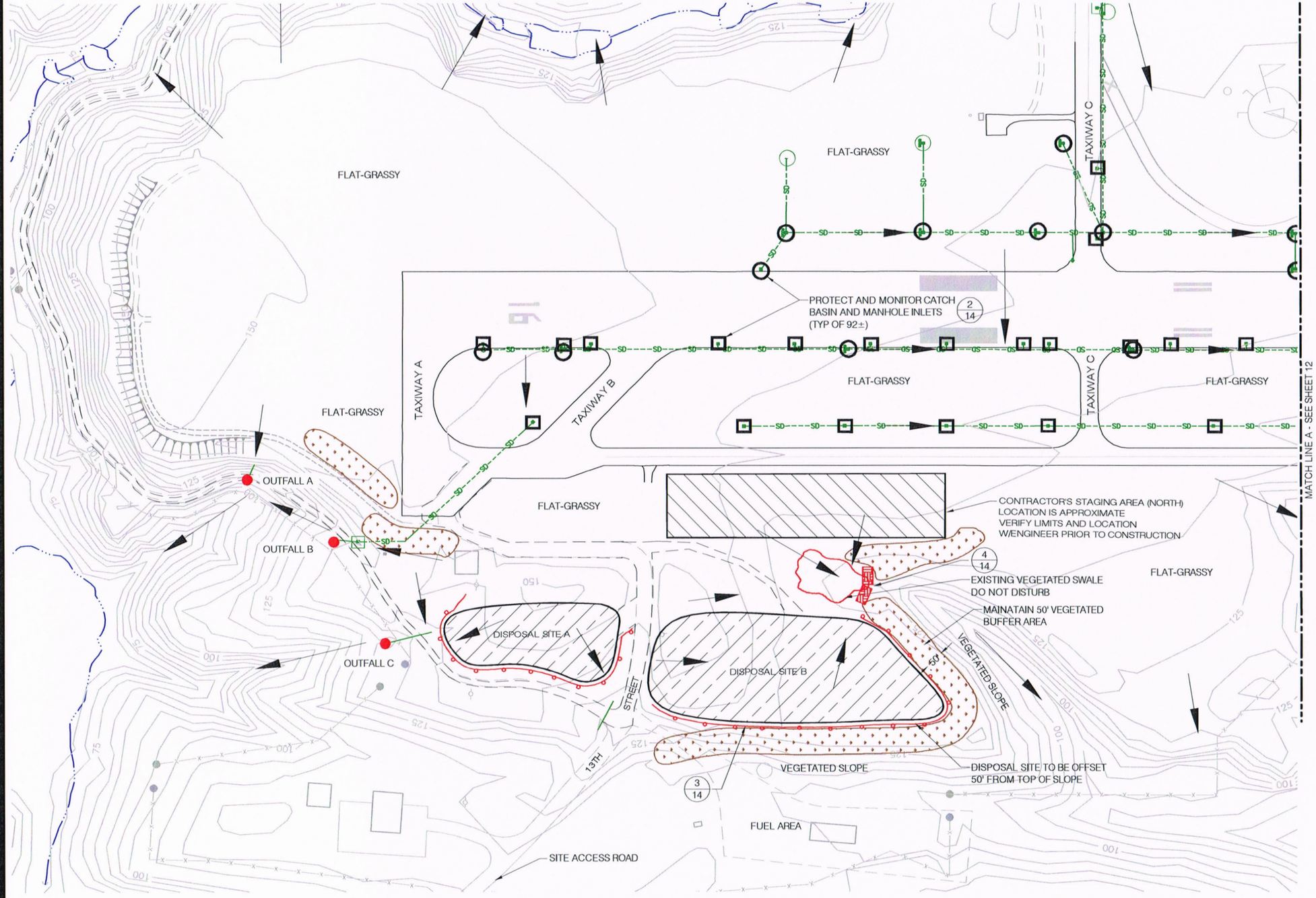
NEWPORT MUNICIPAL AIRPORT
RUNWAY 16-34 REHABILITATION

ESCP SITE LAYOUT

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: **NPT008**

SHEET NO. **10** OF **108**



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SCALE IN FEET

LEGEND

- PROJECT IMPACT BOUNDARY
- CONSTRUCTION BOUNDARY
- DIRECTION OF FLOW
- (E) CREEK
- TRAVEL WAY
- DRAINAGE OUTFALL
- (E) STORM DRAIN
- (E) CATCH BASIN TO BE PROTECTED AND MONITORED
- (E) STORM MANHOLE TO BE PROTECTED AND MONITORED
- (E) CATCH BASIN TO BE MONITORED AND PROTECTED IF NEEDED
- (E) MANHOLE TO BE MONITORED AND PROTECTED IF NEEDED
- SEDIMENT FENCE
- MINIMUM 50' VEGETATED BUFFER AREA
- (E) VEGETATED DRAINAGE SWALE
- HAY BALE BARRIER

NOTES:

SEE SHEETS 10 AND 15.

REGISTERED PROFESSIONAL ENGINEER
11,146
OREGON
JULY 17, 1981
RONALD F. STILLMAKER
EXPIRES 06/30/14

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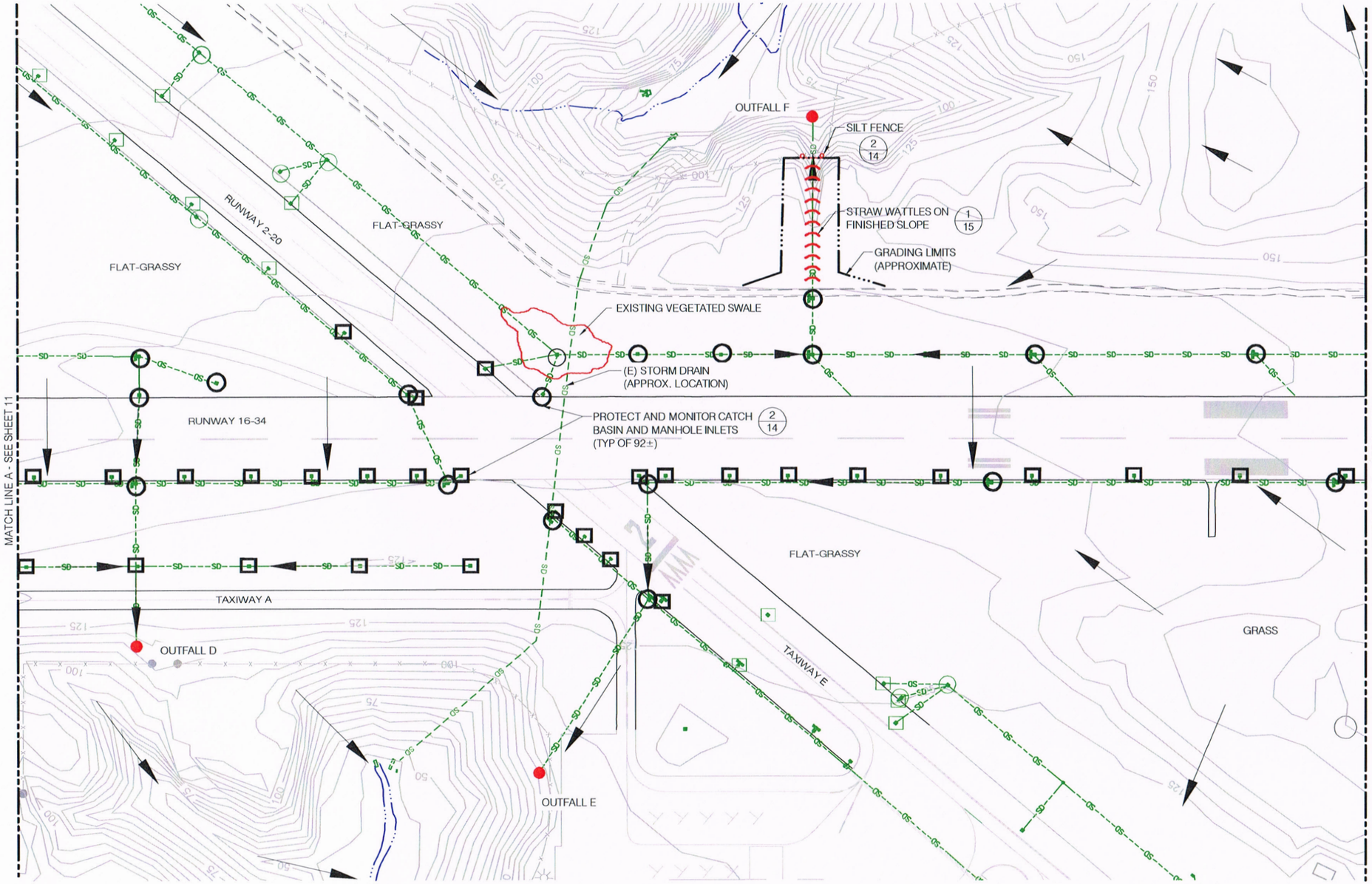
NEWPORT MUNICIPAL AIRPORT
RUNWAY 16-34 REHABILITATION

ESCP BMPS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: NPT008

SHEET NO. 11 OF 108



NORTH

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SCALE IN FEET

LEGEND

- PROJECT IMPACT BOUNDARY
- CONSTRUCTION BOUNDARY
- DIRECTION OF FLOW
- (E) CREEK
- TRAVEL WAY
- DRAINAGE OUTFALL
- (E) STORM DRAIN
- (E) CATCH BASIN TO BE PROTECTED AND MONITORED
- (E) STORM MANHOLE TO BE PROTECTED AND MONITORED
- (E) CATCH BASIN TO BE MONITORED AND PROTECTED IF NEEDED
- (E) MANHOLE TO BE MONITORED AND PROTECTED IF NEEDED
- SEDIMENT FENCE
- MINIMUM 50' VEGETATED BUFFER AREA
- (E) VEGETATED DRAINAGE SWALE
- STRAW WATTLE

NOTES:

SEE SHEETS 10 AND 15.

REGISTERED PROFESSIONAL ENGINEER
11,146
OREGON
JULY 17, 1981
RONALD F. STILLMAKER
EXPIRES 06/30/14

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NEWPORT MUNICIPAL AIRPORT
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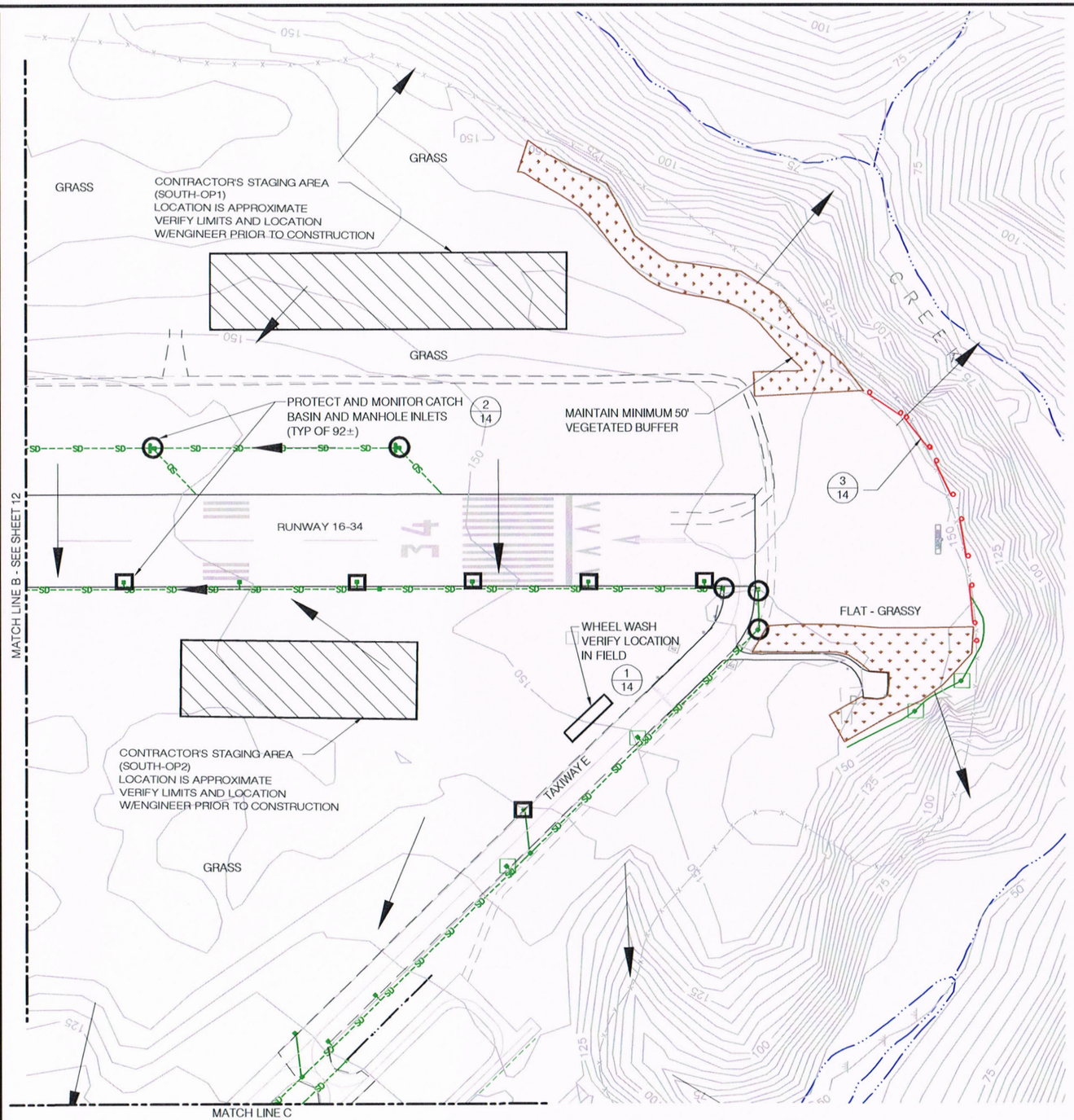
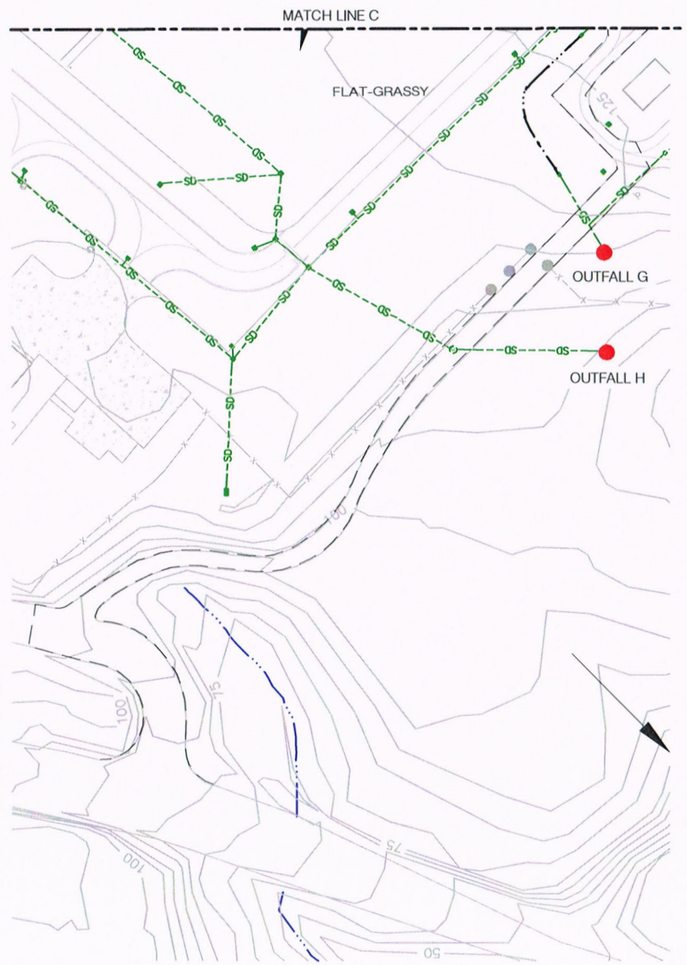
ESCP - BMPS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: NPT008

SHEET NO. 12 OF 108





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SCALE IN FEET

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- CONSTRUCTION BOUNDARY
- DIRECTION OF FLOW
- (E) CREEK
- TRAVEL WAY
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- (E) STORM DRAIN
- (E) CATCH BASIN TO BE PROTECTED AND MONITORED
- (E) STORM MANHOLE TO BE PROTECTED AND MONITORED
- (E) CATCH BASIN TO BE MONITORED AND PROTECTED IF NEEDED
- (E) MANHOLE TO BE MONITORED AND PROTECTED IF NEEDED
- SEDIMENT FENCE
- MINIMUM 50' VEGETATED BUFFER AREA
- (E) VEGETATED DRAINAGE SWALE

NOTES:

1. STAGING AREA SOUTH-OP1 OR SOUTH-OP2 TO BE DETERMINED BY ENGINEER AND CONTRACTOR.
2. SEE NOTES ON SHEETS 10 AND 15.

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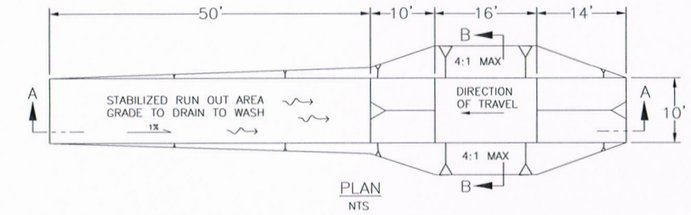
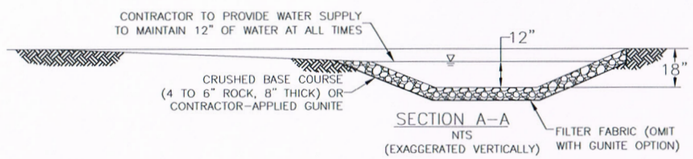
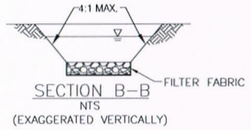
SHEET NO. 13 OF 108



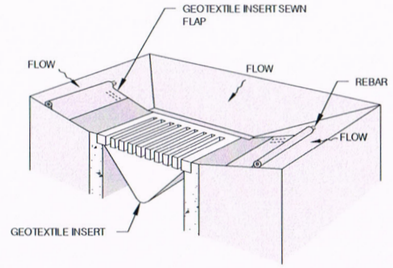


NOTE:
1. CONTRACTOR TO REMOVE ACCUMULATED SEDIMENT FROM WHEEL WASH. MAY BE PIPED TO AN APPROVED SEDIMENT TRAP.

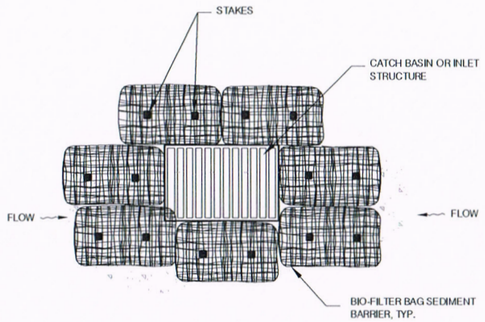
REFERENCE:
USE GEOTEXTILE FABRIC WITH AGGREGATE FOR A TEMPORARY TIRE WASH.



WHEEL WASH DETAIL 1/14
NTS



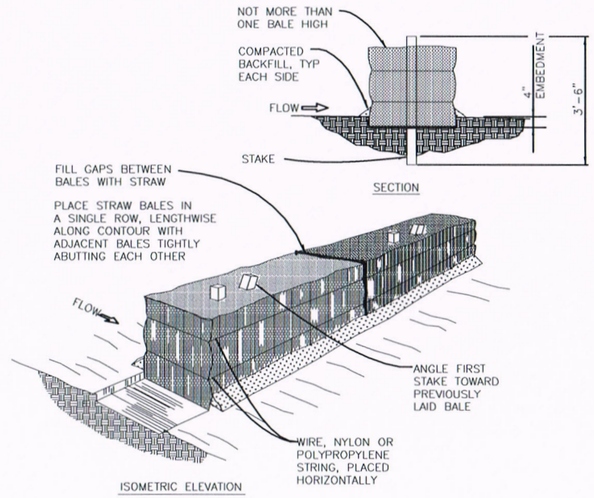
NOTES:
1. REMOVE SEDIMENT FROM INSERT BEFORE IT REACHES 50% CAPACITY.
2. BOTH GEOTEXTILE INSERT AND BIO-FILTER BAG SEDIMENT BARRIER TO BE USED ON ALL DESIGNATED INLETS.



INLET PROTECTION DETAIL 2/14
NTS

NOTES:
1. INSTALLATION SHALL FOLLOW THE MANUFACTURER'S RECOMMENDATIONS.
2. POSTS SHALL BE INSTALLED SO THAT A MIN. OF 3'-0" EXTENDS ABOVE THE GROUND WITH A MIN. OF 2'-0" EMBEDMENT.
3. FILTER FABRIC SHALL BE PURCHASED IN A CONTINUOUS ROLL TO AVOID JOINTS.
4. SPLICE JOINTS AT SUPPORT POSTS ONLY, WITH A MIN. 6" OVERLAP.
5. SEDIMENT FENCE SHALL BE HIGH VISIBILITY ORANGE FENCE.
6. REMOVED TRAPPED SEDIMENT BEFORE IT REACHES 1/3 OF THE ABOVE GROUND FENCE HEIGHT.

SEDIMENT FENCE DETAIL 3/14
NTS



STRAW BALE BARRIER DETAIL 4/14
NTS

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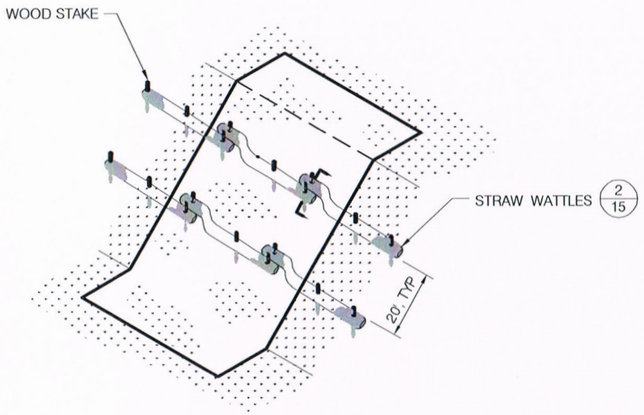
NEWPORT MUNICIPAL AIRPORT
RUNWAY 16-34 REHABILITATION

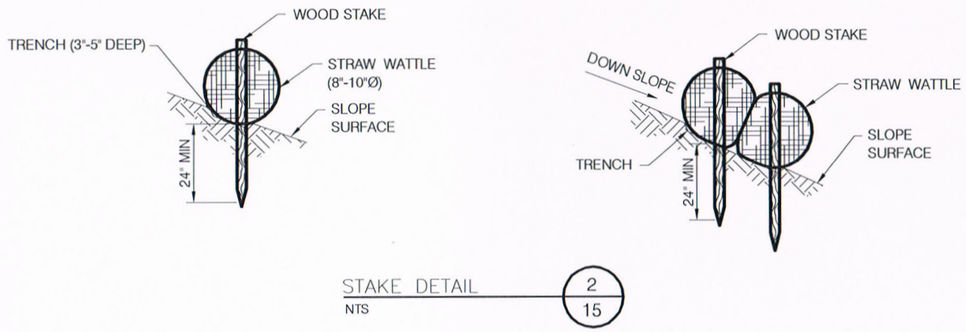
ESCP DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

PROJECT NUMBER: **NPT008**
SHEET NO. **14** OF **108**



SLOPE DETAIL 1/15
NTS



STAKE DETAIL 2/15
NTS

ESCP DRAWING STANDARD NOTES

1. HOLD A PRE-CONSTRUCTION MEETING OF PROJECT CONSTRUCTION PERSONNEL THAT INCLUDES THE INSPECTOR TO DISCUSS EROSION AND SEDIMENT CONTROL MEASURES AND CONSTRUCTION LIMITS. (SCHEDULE A.8.C.I.(3))
2. ALL PERMIT REGISTRANTS MUST IMPLEMENT THE ESCP. FAILURE TO IMPLEMENT ANY OF THE CONTROL MEASURES OR PRACTICES DESCRIBED IN THE ESCP IS A VIOLATION OF THE PERMIT. (SCHEDULE A 8.A)
3. RETAIN A COPY OF THE ESCP AND ALL REVISIONS ON SITE AND MAKE IT AVAILABLE ON REQUEST TO DEQ, AGENT, OR THE LOCAL MUNICIPALITY. DURING INACTIVE PERIODS OF GREATER THAN SEVEN (7) CONSECUTIVE CALENDAR DAYS, RETAIN THE ESCP AT THE CONSTRUCTION SITE OR AT ANOTHER LOCATION. (SCHEDULE B.2.A)
4. THE ESCP MEASURES SHOWN ON THIS PLAN ARE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, UPGRADE THESE MEASURES AS NEEDED TO COMPLY WITH ALL APPLICABLE LOCAL, STATE, AND FEDERAL EROSION AND SEDIMENT CONTROL REGULATIONS. (SCHEDULE A.B.C.II.(1)(C))
5. SUBMISSION OF ALL ESCP REVISIONS IS NOT REQUIRED. SUBMITTAL OF THE ESCP REVISIONS IS ONLY UNDER SPECIFIC CONDITIONS. (SCHEDULE A.12.C.III)
6. PHASE CLEARING AND GRADING TO THE MAXIMUM EXTENT PRACTICAL TO PREVENT EXPOSED INACTIVE AREAS FROM BECOMING A SOURCE OF EROSION. (SCHEDULE A 8.C.II.(1)(D))
7. IDENTIFY, MARK, AND PROTECT (BY FENCING OFF OR OTHER MEANS) CRITICAL RIPARIAN AREAS AND VEGETATION INCLUDING IMPORTANT TREES AND ASSOCIATED ROOTING ZONES, AND VEGETATION AREAS TO BE PRESERVED. IDENTIFY VEGETATIVE BUFFER ZONES BETWEEN THE SITE AND SENSITIVE AREAS (E.G., WETLANDS), AND OTHER AREAS TO BE PRESERVED, ESPECIALLY IN PERIMETER AREAS. (SCHEDULE A.8.C.I.(1) & (2))
8. PRESERVE EXISTING VEGETATION AND RE-VEGETATE OPEN AREAS WHEN PRACTICABLE BEFORE AND AFTER GRADING OR CONSTRUCTION. (SCHEDULE A.7.B.III.(1))
9. EROSION AND SEDIMENT CONTROL MEASURES INCLUDING PERIMETER SEDIMENT CONTROL MUST BE IN PLACE BEFORE VEGETATION IS DISTURBED AND MUST REMAIN IN PLACE AND BE MAINTAINED, REPAIRED, AND PROMPTLY IMPLEMENTED FOLLOWING PROCEDURES
ESTABLISHED FOR THE DURATION OF CONSTRUCTION, INCLUDING PROTECTION FOR ACTIVE STORM DRAIN INLETS AND CATCH BASINS AND APPROPRIATE NON-STORMWATER POLLUTION CONTROLS. (SCHEDULE A.7.D.I AND A.8.C)
10. ESTABLISH CONCRETE TRUCK AND OTHER CONCRETE EQUIPMENT WASHOUT AREAS BEFORE BEGINNING CONCRETE WORK. (SCHEDULE A.8.C.I.(6))
11. APPLY TEMPORARY AND/OR PERMANENT SOIL STABILIZATION MEASURES IMMEDIATELY ON ALL DISTURBED AREAS AS GRADING PROGRESSES AND FOR ALL ROADWAYS INCLUDING GRAVEL ROADWAYS. (SCHEDULE A.8.C.II.(2))
12. ESTABLISH MATERIAL AND WASTE STORAGE AREAS, AND OTHER NON-STORMWATER CONTROLS. (SCHEDULE A.B.C.I.(7))
13. PREVENT TRACKING OF SEDIMENT ONTO PUBLIC OR PRIVATE ROADS USING BMPS SUCH AS: GRAVELED (OR PAVED) EXITS AND PARKING AREAS, GRAVEL ALL UNPAVED ROADS LOCATED ONSITE, OR USE AN EXIT TIRE WASH. THESE BMPS MUST BE IN PLACE PRIOR TO LAND-DISTURBING ACTIVITIES. (SCHEDULE A 7.D.II.(1) AND A.8.C.I(4))
14. WHEN TRUCKING SATURATED SOILS FROM THE SITE, EITHER USE WATER-TIGHT TRUCKS OR DRAIN LOADS ON SITE. (SCHEDULE A.7.D.II. (3ONCE EVERY TWO (2) WEEKS DURING INACTIVE PERIODS GREATER THAN SEVEN (7) CONSECUTIVE CALENDAR DAYS. (SCHEDULE B.1.B.(2) & (3))
15. USE BMPS TO PREVENT OR MINIMIZE STORMWATER EXPOSURE TO POLLUTANTS FROM SPILLS; VEHICLE AND EQUIPMENT FUELING, MAINTENANCE, AND STORAGE; OTHER CLEANING AND MAINTENANCE ACTIVITIES; AND WASTE HANDLING ACTIVITIES. THESE POLLUTANTS INCLUDE FUEL, HYDRAULIC FLUID, AND OTHER OILS FROM VEHICLES AND MACHINERY, AS WELL AS DEBRIS, LEFTOVER PAINTS, SOLVENTS, AND GLUES FROM CONSTRUCTION OPERATIONS. (SCHEDULE A.7.E.I.(2))
16. WATER OR USE A SOIL-BINDING AGENT OR OTHER DUST CONTROL TECHNIQUE AS NEEDED TO AVOID WIND-BLOWN SOIL. (SCHEDULE A.7.B.III)
17. THE APPLICATION RATE OF FERTILIZERS USED TO REESTABLISH VEGETATION MUST FOLLOW MANUFACTURER'S RECOMMENDATIONS TO MINIMIZE NUTRIENT RELEASES TO SURFACE WATERS. EXERCISE CAUTION WHEN USING TIME-RELEASE FERTILIZERS WITHIN ANY WATERWAY RIPARIAN ZONE. (SCHEDULE A.9.B.III)
18. IF A STORMWATER TREATMENT SYSTEM (FOR EXAMPLE, ELECTRO-COAGULATION, FLOCCULATION, FILTRATION, ETC.) FOR SEDIMENT OR OTHER POLLUTANT REMOVAL IS EMPLOYED, SUBMIT AN OPERATION AND MAINTENANCE PLAN (INCLUDING SYSTEM SCHEMATIC, LOCATION OF SYSTEM, LOCATION OF INLET, LOCATION OF DISCHARGE, DISCHARGE DISPERSION DEVICE DESIGN, AND A SAMPLING PLAN AND FREQUENCY) BEFORE OPERATING THE TREATMENT SYSTEM. OBTAIN PLAN APPROVAL BEFORE OPERATING THE TREATMENT SYSTEM. OPERATE AND MAINTAIN THE TREATMENT SYSTEM ACCORDING TO MANUFACTURER'S SPECIFICATIONS. (SCHEDULE A.9.D)
19. TEMPORARILY STABILIZE SOILS AT THE END OF THE SHIFT BEFORE HOLIDAYS AND WEEKENDS, IF NEEDED. THE REGISTRANT IS RESPONSIBLE FOR ENSURING THAT SOILS ARE STABLE DURING RAIN EVENTS AT ALL TIMES OF THE YEAR. (SCHEDULE A 7.B)
20. CONSTRUCTION ACTIVITIES MUST AVOID OR MINIMIZE EXCAVATION AND CREATION OF BARE GROUND DURING WET WEATHER. (SCHEDULE A.7.A.I)
21. SEDIMENT FENCE: REMOVE TRAPPED SEDIMENT BEFORE IT REACHES ONE THIRD OF THE ABOVE GROUND FENCE HEIGHT AND BEFORE FENCE REMOVAL. (SCHEDULE A.9.C.I)
22. OTHER SEDIMENT BARRIERS (SUCH AS BIOBAGS): REMOVE SEDIMENT BEFORE IT REACHES TWO INCHES DEPTH ABOVE GROUND HEIGHT, AND BEFORE BMP REMOVAL. (SCHEDULE A.9.C.II)
23. SEDIMENT BASINS AND SEDIMENT TRAPS: REMOVE TRAPPED SEDIMENTS BEFORE DESIGN CAPACITY HAS BEEN REDUCED BY FIFTY PERCENT AND AT COMPLETION OF PROJECT. (SCHEDULE A.9.C.III & IV)
24. WITHIN 24 HOURS, SIGNIFICANT SEDIMENT THAT HAS LEFT THE CONSTRUCTION SITE, MUST BE REMEDIATED. INVESTIGATE THE CAUSE OF THE SEDIMENT RELEASE AND IMPLEMENT STEPS TO PREVENT A RECURRENCE OF THE DISCHARGE WITHIN THE SAME 24 HOURS. ANY IN-STREAM CLEAN UP OF SEDIMENT SHALL BE PERFORMED ACCORDING TO THE OREGON DIVISION OF STATE LANDS REQUIRED TIMEFRAME. (SCHEDULE A.9.B.I)
25. THE INTENTIONAL WASHING OF SEDIMENT INTO STORM SEWERS OR DRAINAGE WAYS MUST NOT OCCUR. VACUUMING OR DRY SWEEPING AND MATERIAL PICKUP MUST BE USED TO CLEANUP RELEASED SEDIMENTS. (SCHEDULE A.9.B.II)
26. THE ENTIRE SITE MUST BE TEMPORARILY STABILIZED USING VEGETATION OR A HEAVY MULCH LAYER, TEMPORARY SEEDING, OR OTHER METHOD SHOULD ALL CONSTRUCTION ACTIVITIES CEASE FOR 30 DAYS OR MORE. (SCHEDULE A.7.F.I)
27. PROVIDE TEMPORARY STABILIZATION FOR THAT PORTION OF THE SITE WHERE CONSTRUCTION ACTIVITIES CEASE FOR 14 DAYS OR MORE WITH A COVERING OF BLOWN STRAW AND A TACKIFIER, LOOSE STRAW, OR AN ADEQUATE COVERING OF COMPOST MULCH UNTIL WORK RESUMES ON THAT PORTION OF THE SITE. (SCHEDULE A.7.F.II)
28. THE DESIGNATED EROSION AND SEDIMENT CONTROL INSPECTOR MUST PERFORM DAILY INSPECTIONS OF THE BMPS AND DISCHARGE OUTFALLS WHEN RAINFALL AND RUNOFF OCCUR. RECORD THE INSPECTIONS AND OBSERVATIONS IN A LOG THAT IS ON SITE. (SCHEDULE B.1.B(1))
29. ALL ESCP CONTROLS AND PRACTICES MUST BE INSPECTED VISUALLY ONCE TO ENSURE THAT BMPS ARE IN WORKING ORDER PRIOR TO THE SITE BECOMING INACTIVE OR IN ANTICIPATION OF SITE INACCESSIBILITY AND MUST BE INSPECTED VISUALLY
30. IF PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT AND ACCESSIBLE DISCHARGE POINT OR DOWNSTREAM LOCATION DURING PERIODS IN WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER. (SCHEDULE B.1.B.(4))
31. DO NOT REMOVE TEMPORARY SEDIMENT CONTROL PRACTICES UNTIL PERMANENT VEGETATION OR OTHER COVER OF EXPOSED AREAS IS ESTABLISHED. IDENTIFY THE TYPE OF VEGETATIVE SEED MIX USED. (SCHEDULE A 7.B.III)
32. PROVIDE PERMANENT EROSION CONTROL MEASURES ON ALL EXPOSED AREAS. REMOVE ALL TEMPORARY EROSION CONTROL MEASURES AS EXPOSED AREAS BECOME STABILIZED, UNLESS DOING SO CONFLICTS WITH LOCAL REQUIREMENTS. PROPERLY DISPOSE OF CONSTRUCTION MATERIALS AND WASTE, INCLUDING SEDIMENT RETAINED BY TEMPORARY BMPS. (SCHEDULE A.8.C.III)

INSPECTION FREQUENCY

SITE CONDITION	MINIMUM FREQUENCY
1. ACTIVE PERIOD	DAILY WHEN STORMWATER RUNOFF IS OCCURING. AT LEAST ONCE EVERY TWO WEEKS REGARDLESS OF WHETHER OR NOT RUNOFF IS OCCURING.
2. PRIOR TO THE SITE BECOMING INACTIVE OR IN ANTICIPATION OF SITE INACCESSIBILITY	ONCE TO ENSURE THAT EROSION AND SEDIMENT CONTROL MEASURES ARE IN WORKING ORDER. ANY NECESSARY MAINTENANCE AND REPAIR MUST BE MADE PRIOR TO LEAVING THE SITE.
3. INACTIVE PERIODS GREATER THAN FOURTEEN (14) CALENDAR DAYS	ONCE EVERY TWO (2) WEEKS.
4. PERIODS DURING WHICH THE SITE IS INACCESSIBLE DUE TO INCLEMENT WEATHER	IF PRACTICAL, INSPECTIONS MUST OCCUR DAILY AT A RELEVANT AND ACCESSIBLE DISCHARGE POINT OR DOWNSTREAM LOCATION.

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ESCP DETAILS

PRECISION APPROACH ENGINEERING, INC.
AIP NO. 3-41-0040-021

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SHEET NO. 15
OF 108